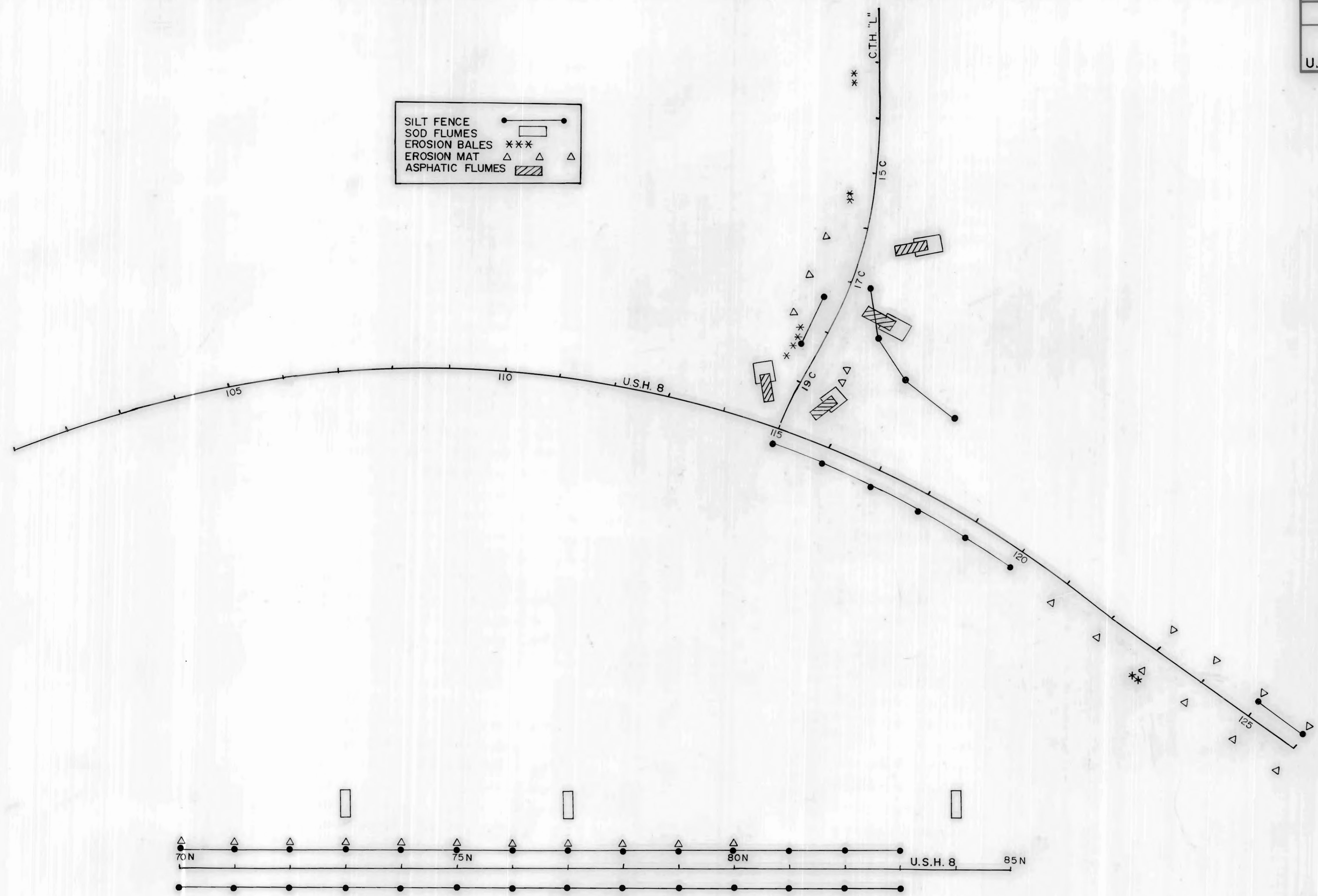


SILT FENCE	—●—●—
SOD FLUMES	□
EROSION BALES	***
EROSION MAT	△ △ △
ASPHTIC FLUMES	▨



SCALE
 1" = 40'



PLOT SCALE:

PLOT NAME:

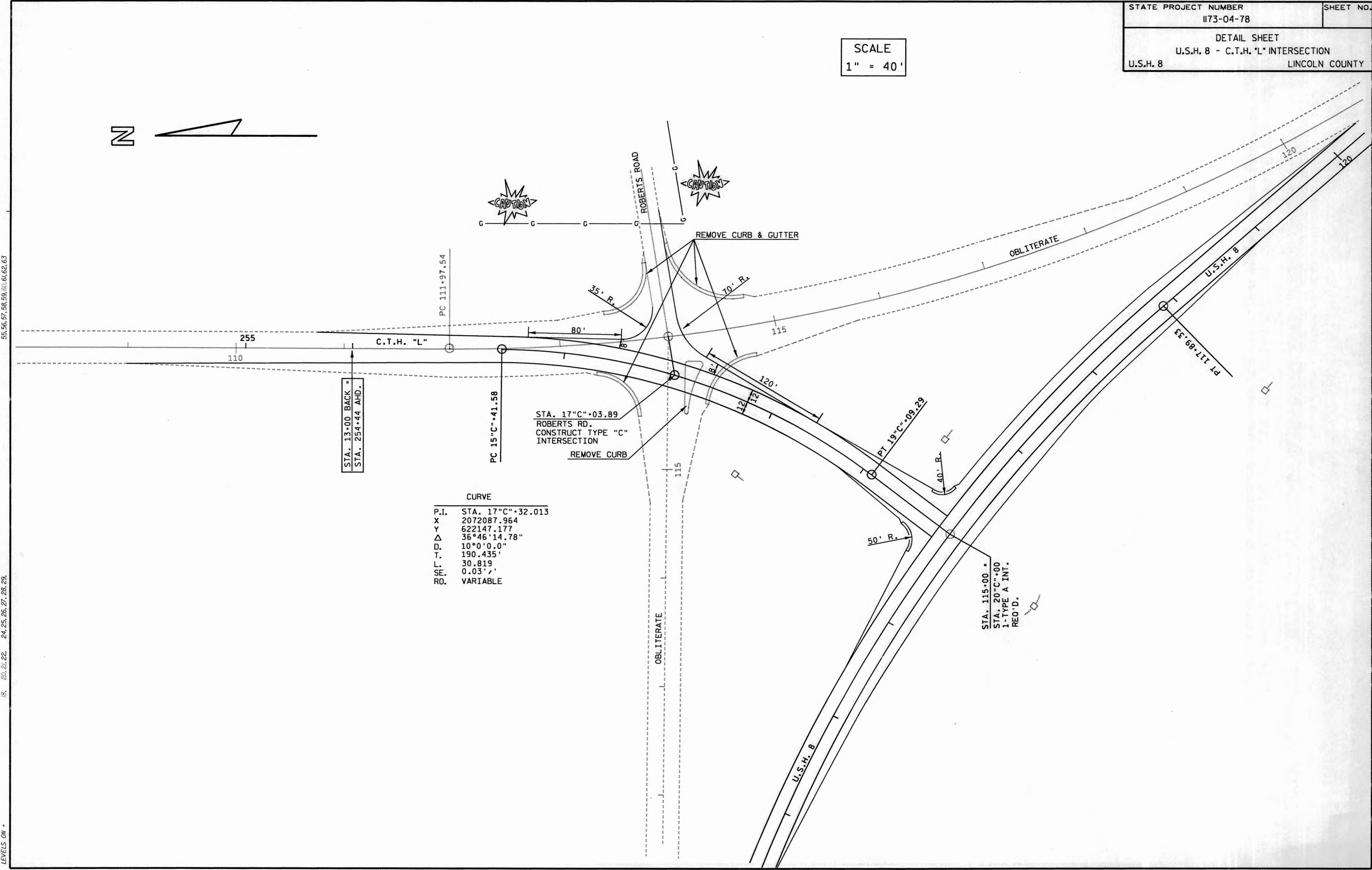
REV. DATE:

ORIGINATOR:

55, 56, 57, 58, 59, 60, 61, 62, 63

18, 20, 21, 22, 24, 25, 26, 27, 28, 29

LEVELS ON *



CURVE

P.I.	STA. 17+00
X	2072087.964
Y	622147.177
Δ	36°46'14.78"
D.	10°0'0.0"
T.	190.435'
L.	30.819
SE.	0.03'
RD.	VARIABLE

INDEX OF SHEETS

Sheet No. 1	Title
Sheet No.	Typical Sections and Details
Sheet No.	Estimate of Quantities
Sheet No.	Miscellaneous Quantities
Sheet No.	Right of Way Plat
Sheet No.	Plan and Profile
Sheet No.	Standard Detail Drawings
Sheet No.	Sign Plates
Sheet No.	Structure Plans
Sheet No.	Computer Earthwork Data
Sheet No.	Cross Sections

TOTAL SHEETS =



DESIGN DESIGNATION

A.D.T.	1992	=	3900
A.D.T.	2012	=	4300
D.H.V.		=	730
D.		=	60-40
T.		=	6%
V.		=	60 MPH
ESALS		=	868700

CONVENTIONAL SIGNS

COUNTY LINE		COMBUSTIBLE FLUIDS (UNDER PRESSURE)	
CORPORATE LIMITS		UNDERGROUND UTILITIES	
PROPERTY LINE		GAS	
LOT LINE		ELECTRIC	
LIMITED HIGHWAY EASEMENT		TELEPHONE	
EXISTING RIGHT OF WAY		SERVICE PEDESTAL	
NEW RIGHT OF WAY		CABLE MARKER	
REFERENCE LINE		POWER POLE	
SLOPE INTERCEPT		TELEPHONE POLE	
ORIGINAL GROUND		RAILROADS	
MARSH OR ROCK PROFILE		MARSH	
CULVERT IN PLACE		WOODED AREA	
CULVERT REQUIRED			
CULVERT REQUIRED (Profile)			

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

U.S.H. 8 - C.T.H. K

HEAFFORD JUNCTION INTERSECTION

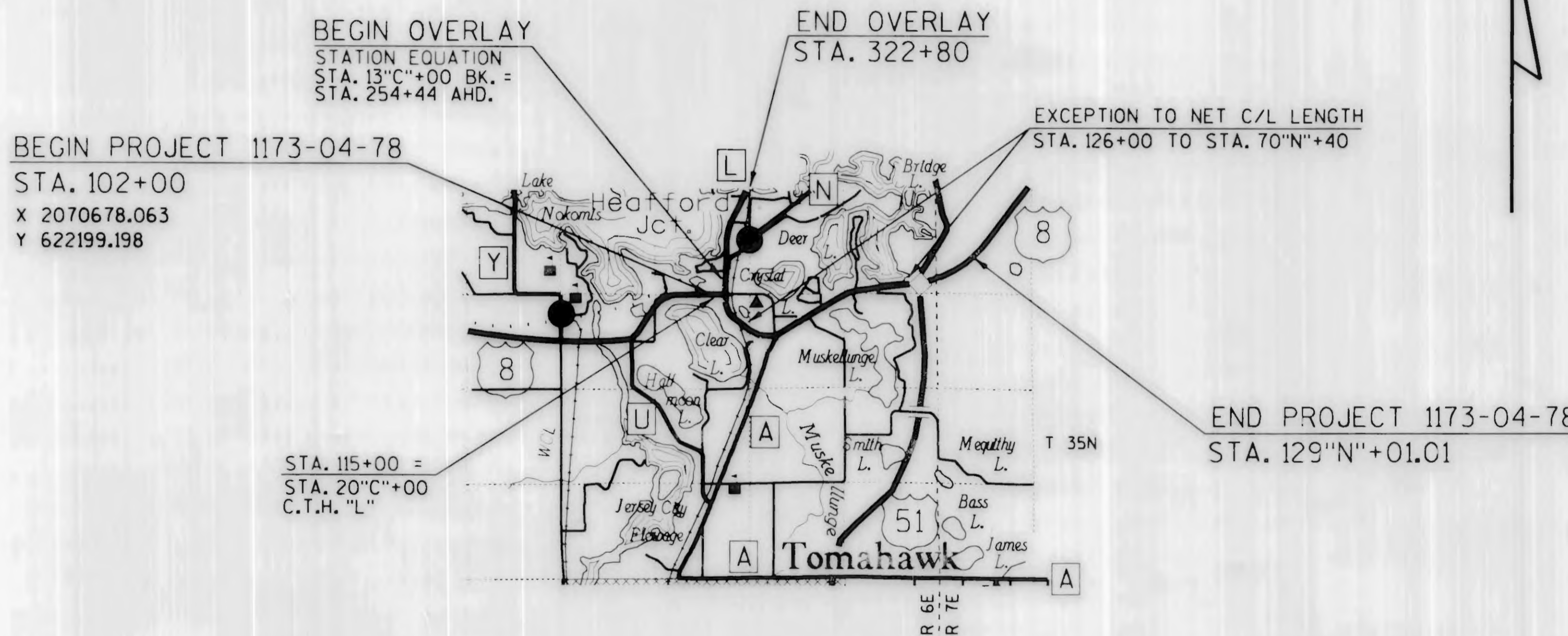
U.S.H. 51

LINCOLN COUNTY

STATE PROJECT NUMBER
1173-04-78



GN



BEGIN PROJECT 1173-04-78
STA. 102+00
* X 2070678.063
* Y 622199.198

BEGIN OVERLAY
STATION EQUATION
STA. 13"C"+00 BK. =
STA. 254+44 AHD.

END OVERLAY
STA. 322+80

EXCEPTION TO NET C/L LENGTH
STA. 126+00 TO STA. 70"N"+40

STA. 115+00 =
STA. 20"C"+00
C.T.H. "L"

END PROJECT 1173-04-78
STA. 129"N"+01.01

LAYOUT
SCALE 0 1 MI.

TOTAL NET LENGTH OF CENTERLINE = 1.550 MI. PROJECT 1173-04-78
1.295 MI. OVERLAY (C.T.H. "L")

* ALL COORDINATES SHOWN ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COORDINATES SYSTEM NORTHERN ZONE

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1173-04-78		

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
Surveyor <u>R.B.J.</u>	District Checker <u>B.P.J.</u>
Designer <u>D.L.J.</u>	C.O. Plan Examiner
District Supervisor <u>A.J.P.</u>	C.O. Coordinator
APPROVED:	
DATE: <u>4/30/92</u>	<u>James D. Bruedler</u> DISTRICT DIRECTOR
APPROVED:	
DATE:	STATE DESIGN ENGINEER FOR HWYS.
U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION REGION 5 WISCONSIN DIVISION	
APPROVED:	
DATE:	DIVISION ADMINISTRATOR

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINAL TOR:

ABBREVIATIONS

AGG.	AGGREGATE	Y	NORTH GRID COORDINATE
AH.	AHEAD	N.B.	NORTHBOUND
∠	ANGLE	NO.	NUMBER
A.C.	ASPHALTIC CEMENT	PAV'T.	PAVEMENT
ASPH.	ASPHALTIC	P.C.	POINT OF CURVATURE
BK.	BACK	P.C.C.	POINT OF CURVE ON CURVE
B.M.	BENCH MARK	P.I.	POINT OF INTERSECTION
B. OR BR.	BRIDGE	P.T.	POINT OF TANGENCY
C.B.	CATCH BASIN	P.O.C.	POINT ON CURVE
C.L. OR C/L	CENTERLINE	P.O.T.	POINT ON TANGENT
Δ	CENTRAL ANGLE OR DELTA	P.V.C.	PORTLAND CEMENT CONCRETE
C.E.	COMMERCIAL ENTRANCE	LB.	POUND
CONC.	CONCRETE	P.E.	PRIVATE ENTRANCE
C.A.C.P.	CORRUGATED ALUMINUM CULVERT PIPE	R.	RADIUS
C.S.C.P.	CORRUGATED STEEL CULVERT PIPE	R.R.	RAILROAD
CO.	COUNTY	R.	RANGE
C.T.H.	COUNTY TRUNK HIGHWAY	R.L. OR R/L	REFERENCE LINE
CR.	CREEK	R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
C.Y. OR CU.YD.	CUBIC YARD	REQ'D.	REQUIRED
C.P.	CULVERT PIPE	RT.	RIGHT
C. & G.	CURB AND GUTTER	R.H.F.	RIGHT HAND FORWARD
D.	DEGREE OF CURVE	R/W	RIGHT-OF-WAY
D.H.V.	DESIGN HOUR VOLUME	R.	RIVER
DIA.	DIAMETER	RD.	ROAD
EA.	EACH	S.S.	SANITARY SEWER
E.	EAST	SHLDR.	SHOULDER
X	EAST GRID COORDINATE	SHR.	SHRINKAGE
E.B.	EASTBOUND	S.	SOUTH
EL. OR ELEV.	ELEVATION	S.B.	SOUTHBOUND
ENT.	ENTRANCE	SO.	SQUARE
EXC.	EXCAVATION	S.F. OR SQ.FT.	SQUARE FEET
E.B.S.	EXCAVATION BELOW SUBGRADE	S.Y. OR SQ.YD.	SQUARE YARD
F.E.	FIELD ENTRANCE	S.T.H.	STATE TRUNK HIGHWAY
G.	GARAGE	STA.	STATION
GRA.	GRAVEL	SS	STORM SEWER
G.N.	GRID NORTH	S.E.	SUPERELEVATION
H.	HOUSE	T.	TANGENT
CWT.	HUNDREDWEIGHT	TEL.	TELEPHONE
IN. DIA.	INCH DIAMETER	TEMP.	TEMPORARY
INT.	INTERSECTION	T. OR TN.	TOWN
LA.	LANE	T.L. OR T/L	TRANSIT LINE
L.H.F.	LEFT HAND FORWARD	T.	TRUCKS (PERCENT OF)
L.	LENGTH (OF CURVE)	TYP.	TYPICAL
L.F. OR LIN.FT.	LINEAR FOOT	INCL.	UNCLASSIFIED
L.C.	LONG CHORD OF CURVE	U.S.H.	UNITED STATES HIGHWAY
L.S.	LUMP SUM	VAR.	VARIABLE
M.H.	MANHOLE	V.	VELOCITY OR DESIGN SPEED
MAT'L.	MATERIAL	V.C.	VERTICAL CURVE
N.	NORTH	VOL.	VOLUME
		W.	WEST
			WESTBOUND

UTILITIES

WISCONSIN PUBLIC SERVICE
 P.O. BOX 186
 TOMAHAWK, WISC. 54487
 ATTN: JIM GRAETINGER
 TELE: (715) 453-2177

G.T.E. NORTH INCORPORATED
 8737 Hwy 51 NORTH
 MINOCQUA, WISC. 54548
 ATTN: MILT GOETSCH
 TELE: (715) 356-3202

GENERAL NOTES

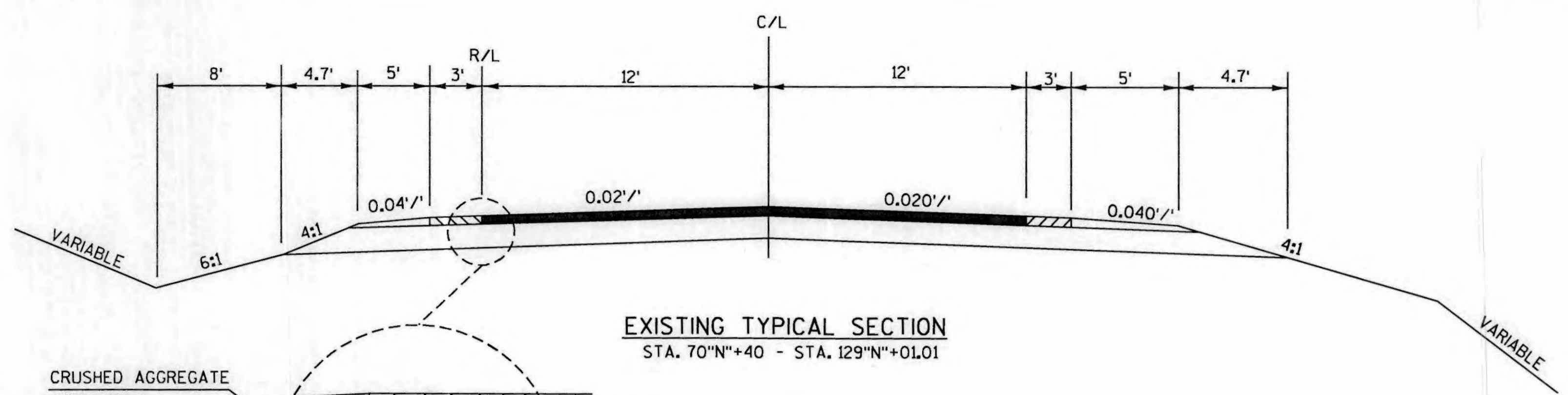
1. WHEN THE QUANTITY OF THE ITEM OF SUBBASE, BASE COURSE OR ASPHALTIC CONCRETE PAVEMENT IS MEASURED BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLAN IS APPROXIMATE; THE ACTUAL THICKNESS WILL DEPEND UPON THE DISTRIBUTION OF THE MATERIALS AS DIRECTED BY THE ENGINEER IN THE FIELD.
2. LENGTH OF RUNOFF SHALL BE COMPUTED WITH TWO-THIRDS OF THE TOTAL RUNOFF ON THE TANGENT APPROACH AND ONE-THIRD WITHIN THE CURVE. SEE PLAN FOR RATE OF SUPERELEVATION.
3. PRIVATE ENTRANCES SHALL BE RESTORED, IN KIND, TO EXISTING CONDITIONS AS DIRECTED BY THE ENGINEER. LIMITS OF RESTORATION TO BE DETERMINED BY THE ENGINEER.
4. TOPSOIL SHALL BE PLACED AT A UNIFORM DEPTH OF 3-INCHES OR AS DIRECTED BY THE ENGINEER.
5. THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
6. EXCAVATION BELOW SUBGRADE (E.B.S.) IS SHOWN ON THE CROSS-SECTIONS, AND IS MEASURED AND PAID FOR AS UNCLASSIFIED EXCAVATION. THE LOCATION OF (E.B.S.) WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
7. SILT FENCE SHALL MEET REQUIREMENTS SPECIFIED FOR SILTY TYPE SOIL.
8. 5-INCH ASPHALTIC CONCRETE PAVEMENT TO BE CONSTRUCTED IN 3 LAYERS.
9. ASPHALTIC SOD DISCHARGE FLUMES ARE REQUIRED AT DISCHARGE POINTS OF CURB AND GUTTER ON RURAL INTERSECTIONS.
10. ALL ASPHALTIC SIDEROADS, PRIVATE AND COMMERCIAL ENTRANCES WILL BE MATCHED WITH A SAWED JOINT EXCEPT WHEN IN A DETERIORATED CONDITION.
11. DIMENSIONS DEPICTING CURB & GUTTER LOCATIONS ARE TO THE FLANGE.

STANDARD DETAIL DRAWINGS

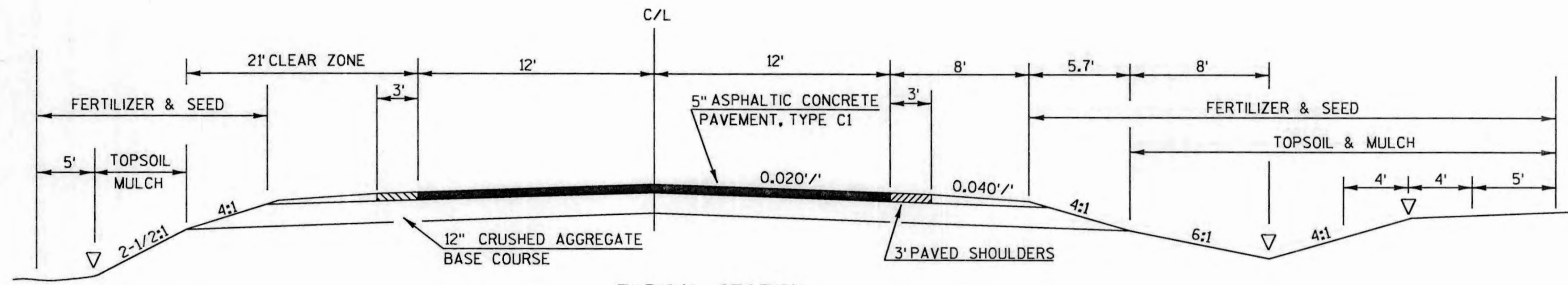
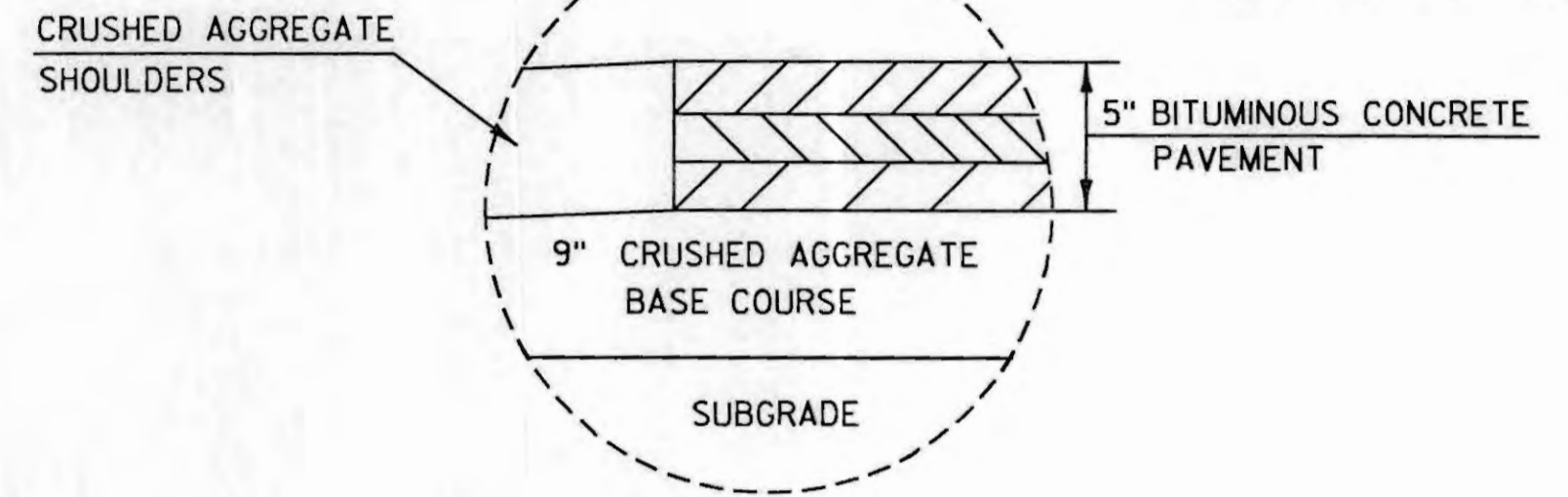
8D1-11	CONCRETE CURB, CONCRETE CURB & GUTTER AND PAVEMENT TIES
8D4-2	CONCRETE SURFACE DRAIN & BITUMINOUS FLUME
8E4-2	SOD OR MASONARY AND SOD DITCH CHECKS
8E7-1	EROSION MAT
8E9-3	SILT FENCE
8F1-10a	APRON ENDWALLS FOR CULVERT PIPE
8F4-3	JOINT TIES FOR CULVERT PIPE
8F6-1	REINFORCED CONCRETE APRON ENDWALL FOR PIPE UNDERDRAIN
9A1-8a	AT GRADE SIDEROAD INTERSECTIONS, TYPE "B", "C" & "D"
9A1-8b	AT GRADE SIDEROAD INTERSECTIONS, TYPE "A" AND PASSING LANES
15C2-2	BARRICADES AND TRAFFIC CONTROL FOR ROAD CLOSURES
15C8-3	PAVEMENT MARKING
15C12-1	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

LEVELS ON - 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63

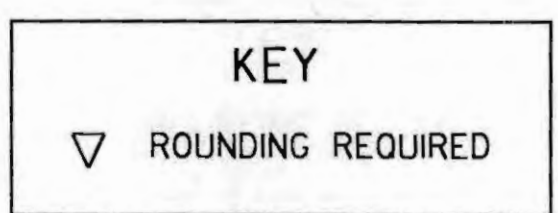
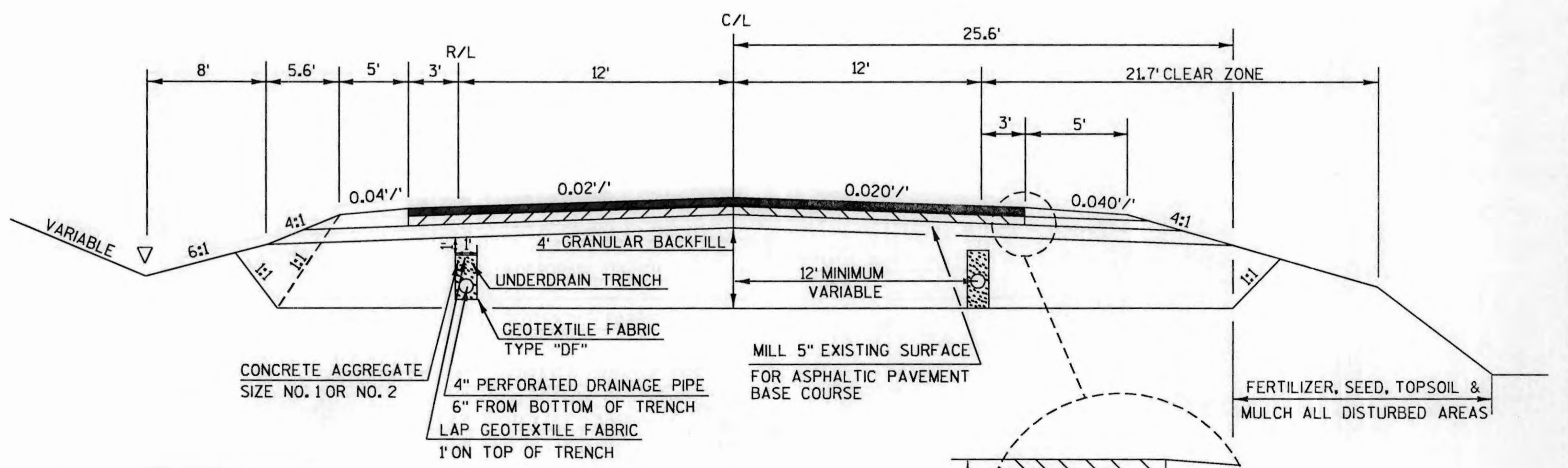
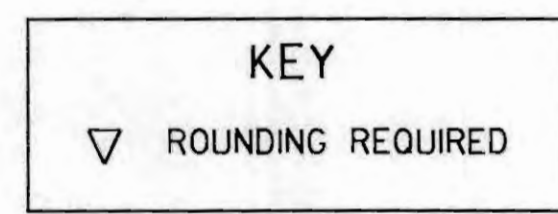
REV. DATE: PLOT NAME: PLOT SCALE:



EXISTING TYPICAL SECTION
STA. 70''N''+40 - STA. 129''N''+01.01

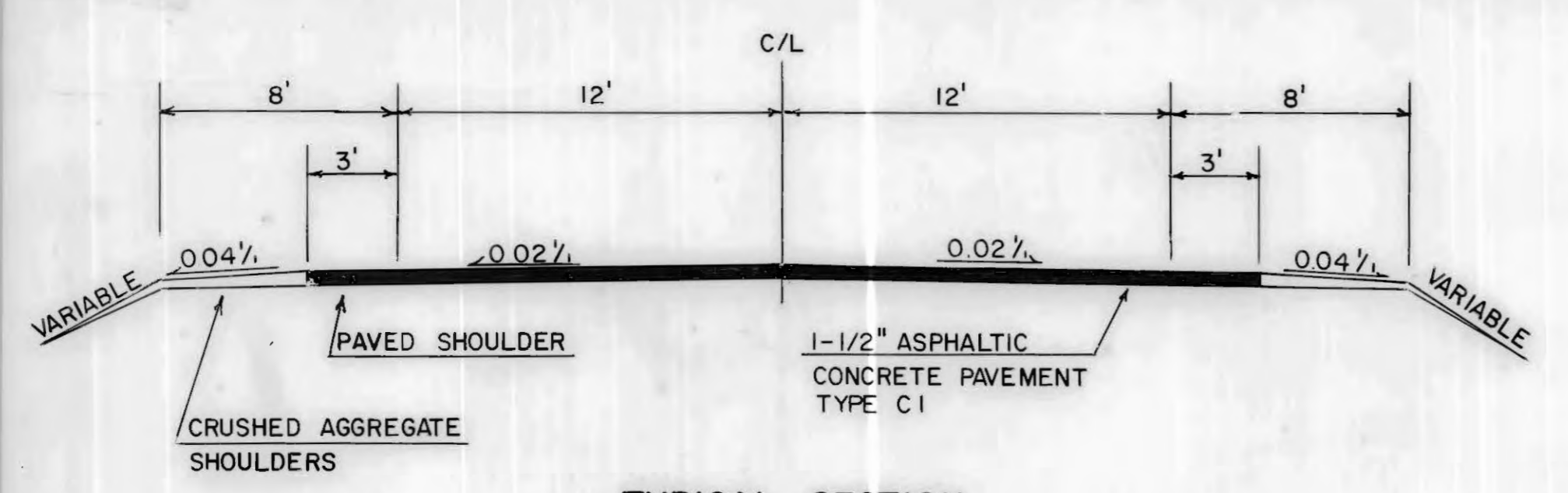


TYPICAL SECTION
STA. 102+00 - STA. 126+00
STA. 13''C''+00 - STA. 19''C''+76

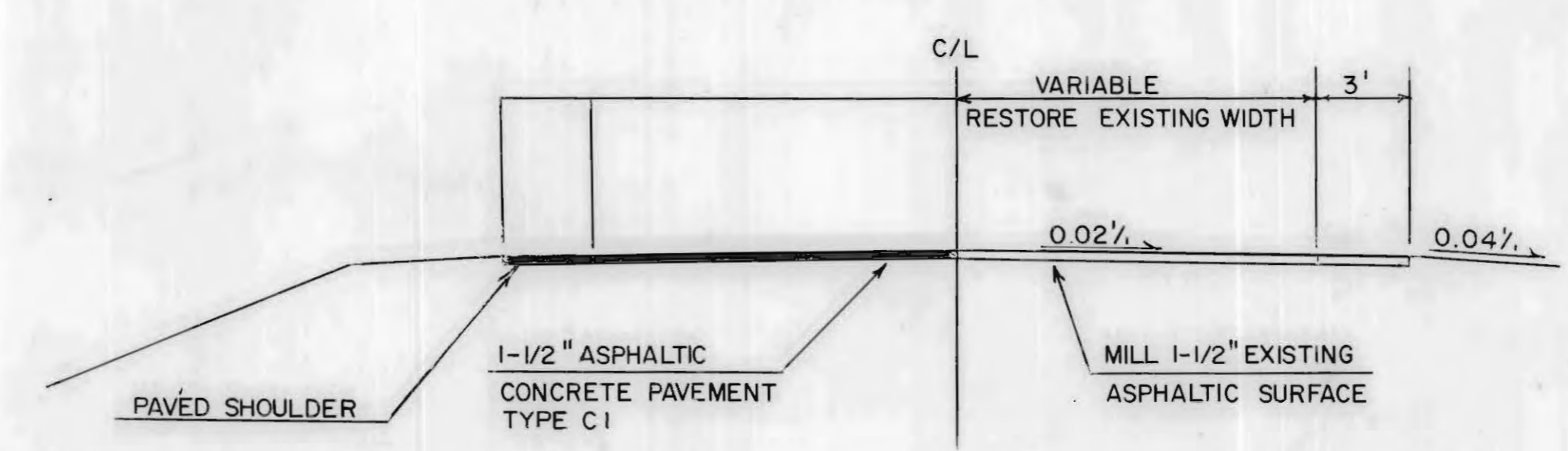


TYPICAL SECTION
STA. 70''N''+40 - STA. 90''N''+00

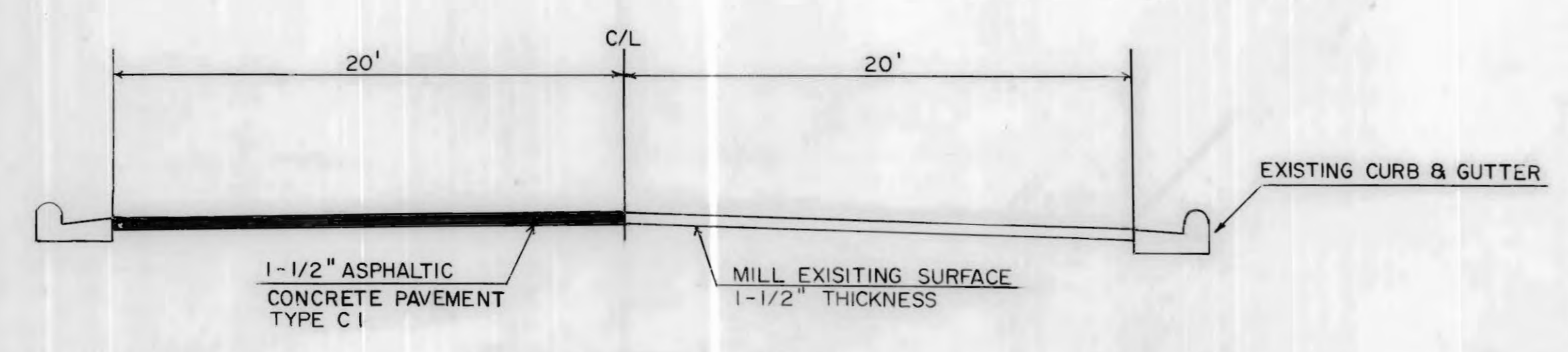
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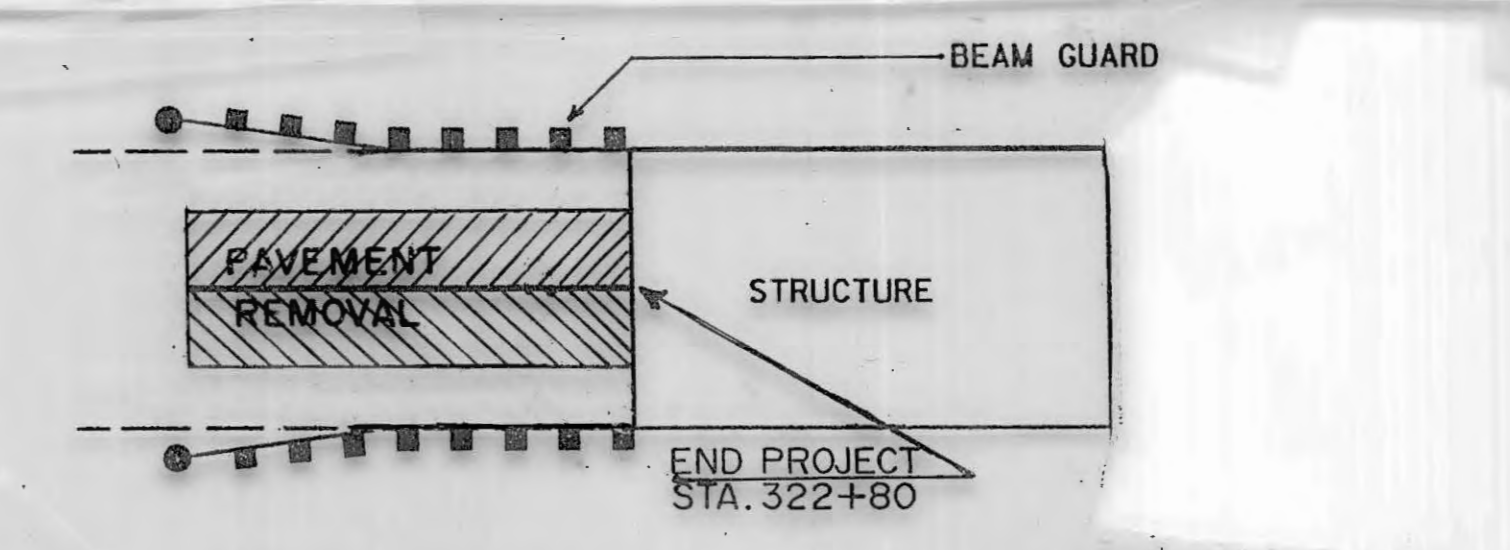
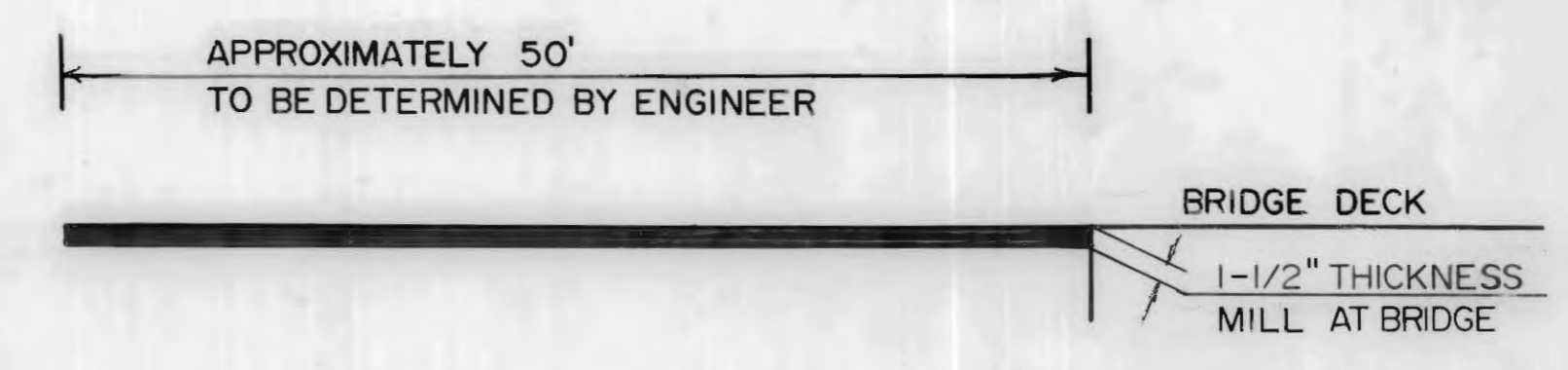
TYPICAL SECTION
 STA 254+44 - STA 265+00
 STA 274+00 - STA 322+80



TYPICAL SECTION
 STA. 90"N"+00 - STA. 129"N"+01.01



TYPICAL SECTION
 STA 265+00 - STA 274+00



DETAIL FOR ASPHALTIC PAVEMENT REPLACEMENT

GN

PHASE II

CONSTRUCT STA. 102+00 - STA. 110+00 TO MOVE TRAFFIC TO NEW ASPHALTIC PAVEMENT ACCORDING TO TRAFFIC CONTROL PLAN AND THE SPECIAL PROVISION TIME REQUIREMENT. REMOVE EXISTING PAVEMENT MARKINGS.

PHASE III

WITH TRAFFIC USING NEW TEMPORARY CONNECTION STA. 118+75 LT. AND THE OLD HIGHWAY NORTH TO STA. 13"C"+00. CONSTRUCT STA. 16"C"+00 TO STA. 19"C"+50 USING MATERIAL FROM OLD ROAD STA. 110+00 - STA. 115+00. EXTEND EXISTING CULVERT PIPE ACROSS NEW APPROACH ACCORDING TO PLAN SHEET APPROXIMATELY, STA. 18"C"+50 RIGHT. AVAILABLE MATERIAL FROM OLD ROAD STA. 110+00 - STA. 115+00 LEFT WILL BE PAID FOR AS UNCLASSIFIED EXCAVATION.

PHASE IV

WITH TRAFFIC USING NEW APPROACH STA. 16"C"+00 - STA. 19"C"+50 REMOVE EXISTING ROAD STA. 115+00 - STA. 120+00 LEFT. AND DITCH ACROSS EXISTING ROAD TO DRAIN AREA BETWEEN EXISTING ROAD AND NEW CONSTRUCTION. FOR C.T.H. "L" REMOVE TEMPORARY APPROACH, STA. 118+75 LT. DITCHING AND REMOVAL TO BE PAID FOR AS UNCL. EXC.

PHASE I

DO NOT OBSTRUCT EXISTING TRAFFIC UNTIL NEW ROAD IS COMPLETED, AND BINDER LAYERS OF ASPHALTIC SURFACE ARE IN PLACE ON NEW HIGHWAY STA. 110+00 - STA. 120+00, INCLUDING NEW TEMPORARY CONNECTION TO OLD HIGHWAY STA. 118+75 LT.

PHASE II

CONSTRUCT STA. 120+00 - STA. 126+00 TO MOVE TRAFFIC TO NEW ASPHALTIC PAVEMENT ACCORDING TO TRAFFIC CONTROL PLAN AND THE SPECIAL PROVISION TIME REQUIREMENT. REMOVE EXISTING PAVEMENT MARKING.

51 55.56.57, 58, 59, 60, 61, 62, 63

20, 21, 22, 23, 24, 25, 26, 27, 28, 29

LEVELS ON

CLEARING & GRUBBING

LOCATION	CLEARING STA.	GRUBBING STA.
STA. 105+00 - STA. 117+00	12	12
STA. 18"C"+00 - STA. 20"C"+00	2	2

<u>REMOVING CURB</u>			<u>REMOVING CURB & GUTTER</u>		
LOCATION	L.F.		LOCATION	L.F.	
STA. 17"C"+00	40' RT.	73	STA. 16+50	40' LT.	58
			STA. 16+70	15' RT.	63
			STA. 17+30	80' LT.	88
			STA. 17+40	C/L	57

REINFORCED CONCRETE CULVERT PIPE, CLASS III, 30-INCH

LOCATION	L.F.	ENDWALLS	JOINT TIES
STA. 116+00 C/L	130	2	12

JOINT TIES ARE INCIDENTAL TO COST OF CULVERT PIPE

ASPHALTIC FLUMES

LOCATION	S.Y.
STA. 114+50 LT.	40
STA. 115+60 LT.	40
STA. 16"C"+40 LT.	40
STA. 17"C"+20 RT.	60

OBLITERATING OLD ROAD

LOCATION	STA.
STA. 110+00 - STA. 115+00 LT.	4.8
STA. 115+00 - STA. 118+00 LT.	4.2

SALVAGED ASPHALTIC PAVEMENT, BASE COURSE

LOCATION	TONS
STA. 70"N"+00 - STA. 90"N"+00	2,900

YARDAGE SUMMARY

LOCATION	UNCL. C.Y.	FILL C.Y.	SHG.	WASTE C.Y.	BORROW C.Y.
STA. 102+00 - STA. 126+00	2,532	25,666	25%	-	29,550
STA. 12"C"+00 - STA. 19"C"+75	4,069	10,353	25%	-	8,872
STA. 70"N"+00 - STA. 90"N"+00	7,374	7,374	50%	7,374	-

SHAPING SHOULDERS

LOCATION	STA.
STA. 254+00 - STA. 323+00 LT.	69
STA. 254+00 - STA. 323+00 RT.	69
STA. 90"N"+00 - STA. 110"N"+00 LT.	20
STA. 90"N"+00 - STA. 110"N"+00 RT.	20
STA. 90"N"+00 - STA. 129"N"+00 LT.	39
STA. 90"N"+00 - STA. 129"N"+00 RT.	39

CRUSHED AGGREGATE BASE COURSE

LOCATION	BASE TONS	SHOULDERS TONS
STA. 102+00 - STA. 126+00	8,426	977
STA. 254+44 - STA. 322+80	-	600
STA. 13"C"+00 - STA. 19"C"+50	2,282	309
STA. 70"N"+40 - STA. 90"N"+00	4,063	683
STA. 90"N"+00 - STA. 129"N"+01	-	940
TEMPORARY CONNECTION STA. 118+00 LT.	320	-
TEMPORARY ROADWAY	1,500	-

ASPHALTIC MATERIAL FOR TACK COAT

LOCATION	GAL.
STA. 90"N"+00 - STA. 129"N"+01	425

ASPHALTIC CONCRETE PAVEMENT TYPE C1 & ASPHALTIC MATERIAL FOR PLANT MIXES

LOCATION	PAV'T TON	A.C. TON
STA. 102+00 - STA. 126+00	2,300	133
STA. 254+44 - STA. 322+80	2,400	144
STA. 12"C"+00 - STA. 19"C"+75	760	44
STA. 70"N"+40 - STA. 90"N"+00	1,900	110
STA. 90"N"+00 - STA. 129"N"+01	1,600	96
PASSING LANE & INTERSECTION ENTRANCES	260	15
	280	16

SALVAGED ASPHALTIC PAVEMENT, MILLING

LOCATION	AVE. DEPTH	TON
STA. 70"N"+40 - STA. 90"N"+00	5-1/2"	1,875
STA. 90"N"+00 - STA. 129"N"+01	1-1/2"	1,525
STA. 265+00 - STA. 274+00	1-1/2"	480
STA. 322+30 - STA. 322+80	1-1/2"	20

CONCRETE CURB & GUTTER, 36-INCH, TYPE D

LOCATION	L.F.
STA. 114+34 LT.	80
STA. 115+65 LT.	62

CALCIUM CHLORIDE SURFACE TREATMENT

LOCATION	TON
STA. 102+00 - STA. 110+00	1
STA. 118+00 - STA. 126+00	1
STA. 13"C"+00 - STA. 19"C"+75	2

GRANULAR BACKFILL

LOCATION	C.Y.
STA. 70"N"+00 - STA. 90"N"+00	11,061

EROSION BALES, DELIVERED & INSTALLED

LOCATION	EA.
STA. 12"C"+00 RT.	8
STA. 13"C"+50 RT.	8
STA. 15"C"+50 RT.	8
STA. 18"C"+50 RT.	50
UNDISTRIBUTED	26

EROSION MAT, DELIVERED & INSTALLED

LOCATION	S.Y.
STA. 121+00 - STA. 126+00 RT.	400
STA. 123+00 - STA. 126+00 LT.	200
STA. 16"C"+50 - STA. 18"C"+00 RT.	250
STA. 18"C"+50 LT.	250
STA. 70"N"+00 - STA. 80"N"+00 LT.	800

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR:

59.

51.

11, 12, 13, 14, 15, 16, 17, 18, 19, 20.

LEVELS ON *

CORRUGATED STEEL CULVERT PIPE

LOCATION	DIA. INCHES	LENGTH L.F.	STEEL APRON ENDWALL
STA. 18"C"+00 C/L	36	104 EXTENSION	1

CULVERT PIPE, CLASS III

MINOR SIDEROADS & PRIVATE ENTRANCE PIPES

LOCATION	DIA. INCHES	LENGTH L.F.	THICKNESS (INCHES) STEEL	ALUMINIUM
STA. 104+50 P.E. RT.	18	26	0.064	0.060
STA. 106+20 P.E. RT.	18	26	0.064	0.060
STA. 107+40 P.E. RT.	18	26	0.064	0.060
STA. 109+40 P.E. RT.	18	26	0.064	0.060
STA. 122+67 TN. RD. RT.	18	50	0.064	0.060

SUBGRADE CONSTRUCTION STAKING

LOCATION	STA.
STA. 102+00 - STA. 126+00	24
STA. 14"C"+00 - STA. 19"C"+00	6
STA. 70"N"+00 - STA. 90"N"+00	20

SAWING EXISTING PAVEMENT

LOCATION	L.F.
STA. 126+00	30
STA. 254+44	30
STA. 70"N"+40	30
STA. 83"N"+25 LT.	24
STA. 83"N"+25 RT.	24
STA. 129"N"+01	30
STA. 17"C"+00 (ROBERTS RD.)	24

SODDING

LOCATION	S.Y.
STA. 19"C"+40 LT.	60
STA. 19"C"+40 RT.	60
STA. 73"N"+00 LT.	20
STA. 77"N"+00 LT.	20
STA. 84"N"+00 LT.	20
UNDISTRIBUTED	120

TRAFFIC CONTROL DRUMS

LOCATION	DAYS
STA. 96+00 - STA. 110+00	1,740
STA. 118+00 - STA. 132+00	1,740
STA. 72"N"+00 - STA. 129"N"+00	1,230

TRAFFIC CONTROL WARNING LIGHTS, TYPE C

LOCATION	DAYS
STA. 96+00 - STA. 132+00	900
STA. 70"N"+00 - STA. 129"N"+00	1,260

TEMPORARY PAVEMENT MARKING

LOCATION	L.F.
STA. 96+00 - STA. 98+00	200
STA. 102+00 - STA. 126+00	384
STA. 126+00 - STA. 128+00	200
STA. 254+00 - STA. 322+00	544
STA. 12"C"+00 - STA. 19"C"+00	56
STA. 70"N"+00 - STA. 90"N"+00	320
STA. 90"N"+00 - STA. 115"N"+00	800
STA. 115"N"+00 - STA. 139"N"+00	384

TRAFFIC CONTROL WARNING LIGHTS, TYPE A

LOCATION	DAYS
STA. 96+00 - STA. 132+00	780
STA. 254+00 - STA. 322+00	240
STA. 72"N"+00 - STA. 129"N"+00	510

TRAFFIC CONTROL BARRICADES, TYPE III

LOCATION	DAYS
STA. 102+00 - STA. 126+00	240
STA. 254+00 - STA. 322+00	60
STA. 70"N"+00 - STA. 129"N"+00	630

REMOVING PAVEMENT MARKING

LOCATION	L.F.
STA. 96+00 - STA. 108+00 LT. & RT.	2,600
STA. 121+00 - STA. 129+00 LT. & RT.	2,000
STA. 14"C"+00 - STA. 17"C"+00 LT.	900

PAVEMENT MARKING, HOT PAINT

LOCATION	4-INCH	
	WHITE L.F.	YELLOW L.F.
DETOUR (C.T.H. "A")	44,000	13,700

PAVEMENT MARKING EPOXY, 4-INCH & 8-INCH

LOCATION	4-INCH		8-INCH WHITE L.F.
	WHITE L.F.	YELLOW L.F.	
STA. 102+00 - STA. 126+00	4,600	4,600	-
STA. 254+00 - STA. 322+00	13,600	12,500	-
STA. 115+60 - STA. 117+60 LT.	-	-	200
STA. 13"C"+00 - STA. 19"C"+50	1,300	1,300	-
STA. 70"E"+00 - STA. 85"E"+00	4,500	3,000	-
STA. 85"E"+00 - STA. 115"E"+00	6,000	6,000	-

PIPE UNDERDRAIN, TRENCH & FABRIC

LOCATION	UNDERDRAIN L.F. 4"	TRENCH L.F.	GEOTEXTILE S.Y.
STA. 73+00 - STA. 84+00	2,200	2,200	2,200
STA. 84+25 - STA. 88+00	750	750	750
CROSS OVER STA. 73+00	44	44	44
CROSS OVER STA. 77+00	44	44	44
CROSS OVER STA. 84+25	53	53	53

REQUIRED FITTINGS 4 END PLUGS, 2-45° ELBOWS, 3-45° T'S,
3 RODENT PROTECTED ENDS, 1-45° CROSS PIPE CONNECTION.

TOPSOIL, FERTILIZER, SEED, MULCH & AGRICULTURAL LIMESTONE

LOCATION	TOPSOIL S.Y.	FERTILIZER TYPE B CWT.	SEED NO. 10 LBS.	MULCH S.Y.	AGRICULTURAL LIMESTONE TON
STA. 102+00 - STA. 126+00	10,328	14.4	211	15,660	4.6
STA. 12"C"+00 - STA. 19"C"+75	5,447	5.6	97	7,170	2.4
STA. 70"N"+40 - STA. 90"N"+00	2,225	1.4	60	4,450	1.0
OBLITERATING OLD ROAD	15,000	9.6	202	15,000	6.8
RESTORE TEMPORARY ROAD	2,650	2.0	36	2,650	1.2
BORROW PIT	-	7.0	154	-	-

SILT FENCE, DELIVERED, INSTALLED & MAINTAINED

LOCATION	L.F.
STA. 115+00 - STA. 120+00 RT.	300
STA. 125+00 - STA. 126+00 LT.	150
STA. 17"C"+00 - STA. 19"C"+00 LT.	300
STA. 17"C"+50 - STA. 18"C"+50 RT.	100
STA. 70"N"+00 - STA. 83"N"+00 LT.	1,300
STA. 70"N"+00 - STA. 83"N"+00 RT.	1,300

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR:

59.

51.

LEVELS ON - 11, 12, 13, 14, 15, 16, 17, 18, 19, 20.

SIGNS, TYPE II, REFLECTIVE, TEMPORARY AND SIGN POSTS

DETOUR

SIGN CODE	DESCRIPTION	LOCATION	POSTS 4"x4"x12'	DAYS
○ W20-2	DETOUR 2 MI.	500' W. OF INT. U.S.H. 8 & C.T.H. "U" SOUTH SIDE	1	21
○ W20-2	DETOUR 2 MI.	500' W. OF INT. U.S.H. 8 & C.T.H. "U" NORTH SIDE	1	21
W20-2B	DETOUR 1,500'	1,500' W. OF INT. U.S.H. 8 & C.T.H. "A" SOUTH SIDE	1	21
W20-2B	DETOUR 1,500'	1,500' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE	1	21
M4-8	DETOUR	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE	1	21
M3-2	EAST	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE		21
M1-4	U.S.H. 8	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE		21
M5-1R	ADVANCE TURN RT.	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE		21
M4-8	DETOUR	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" SOUTH SIDE	EXT. POST	21
M5-1R	ADVANCE TURN RT.	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" SOUTH SIDE	EXT. POST	21
M6-1	DIRECTION MARKER	500' W. OF INT. U.S.H. 8 & C.T.H. "A" SOUTH SIDE	EXT. POST	21
* R11-3	ROAD CLOSED 1 MI.	200' E. OF INT. U.S.H. 8 & C.T.H. "A" SOUTH SIDE	1	21
* R11-3	ROAD CLOSED 1 MI.	200' E. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE	1	21
◇ COVER	EXISTING	CITY SIGNS 250' E. OF INT. SOUTH SIDE		
M4-8	DETOUR	S.W. QUAD INT. OF U.S.H. 8 & C.T.H. "A" SOUTH SIDE	EXT. POST	21
M6-1	DIRECTION MARKER	S.W. QUAD INT. OF U.S.H. 8 & C.T.H. "A" SOUTH SIDE	EXT. POST	21
M4-1	DETOUR	S.E. QUAD INT. OF U.S.H. 8 & C.T.H. "A" SOUTH SIDE	1	21
M3-2	EAST	S.E. QUAD INT. OF U.S.H. 8 & C.T.H. "A" SOUTH SIDE		21
M1-4	U.S.H. 8	S.E. QUAD INT. OF U.S.H. 8 & C.T.H. "A" SOUTH SIDE		21
M6-1	DIRECTION MARKER	S.E. QUAD INT. OF U.S.H. 8 & C.T.H. "A" SOUTH SIDE		21
M4-1	DETOUR	100' S. OF INT. U.S.H. 8 & C.T.H. "A" WEST SIDE	1	21
M3-2	EAST	100' S. OF INT. U.S.H. 8 & C.T.H. "A" WEST SIDE		21
M1-4	U.S.H. 8	100' S. OF INT. U.S.H. 8 & C.T.H. "A" WEST SIDE		21
M4-6	END	100' S. OF INT. U.S.H. 8 & C.T.H. "A" EAST SIDE	1	21
M4-8	DETOUR	100' S. OF INT. U.S.H. 8 & C.T.H. "A" EAST SIDE		21
M1-4	U.S.H. 8	100' S. OF INT. U.S.H. 8 & C.T.H. "A" EAST SIDE		21
◇ COVER	EXISTING	EAST U.S.H. 8 & ARROW 100' S. OF INT. U.S.H. 8 & C.T.H. "A"		
◇ COVER	EXISTING	MINOCOQUA & RHINELANDER 150' S. OF INT. U.S.H. 8 & C.T.H. "A" EAST SIDE		
◇ COVER	EXISTING	U.S.H. 8 & EAST ARROW 250' S. OF INT. U.S.H. 8 & C.T.H. "A" EAST SIDE		
M4-8	DETOUR	200' N. OF INT. C.T.H. "A" & C.T.H. "U" EAST SIDE	1	21
M3-4	WEST			21
M1-4	U.S.H. 8			21
M4-8	DETOUR	200' S. OF INT. C.T.H. "A" & C.T.H. "U" WEST SIDE	1	21
M3-4	WEST			21
M1-4	U.S.H. 8			21
M4-8	DETOUR	1,000' N. OF INT. C.T.H. "A" & 4th ST. WEST SIDE	1	21
M3-2	EAST	1,000' N. OF INT. C.T.H. "A" & 4th ST. WEST SIDE		21
M1-4	U.S.H. 8	1,000' N. OF INT. C.T.H. "A" & 4th ST. WEST SIDE		21
M5-1L	ADVANCE TURN LT.	1,000' N. OF INT. C.T.H. "A" & 4th ST. WEST SIDE		21
M4-8	DETOUR	1,000' N. OF INT. C.T.H. "A" & 4th ST. EAST SIDE	1	21
M3-2	EAST	1,000' N. OF INT. C.T.H. "A" & 4th ST. EAST SIDE		21
M1-4	U.S.H. 8	1,000' N. OF INT. C.T.H. "A" & 4th ST. EAST SIDE		21
M5-1L	ADVANCE TURN LT.	1,000' N. OF INT. C.T.H. "A" & 4th ST. EAST SIDE		21
RHINELANDER	WITH ARROW LT.	500' N. OF INT. C.T.H. "A" & 4th ST. WEST SIDE	1	21
M4-8	DETOUR	AT INT. C.T.H. "A" & 4th ST. WEST SIDE	1	21
M3-2	EAST	AT INT. C.T.H. "A" & 4th ST. WEST SIDE		21
M1-4	U.S.H. 8	AT INT. C.T.H. "A" & 4th ST. WEST SIDE		21
M6-1	DIRECTION MARKER	AT INT. C.T.H. "A" & 4th ST. WEST SIDE		21
M4-8	DETOUR	200' N. OF INT. C.T.H. "A" & 4th ST. EAST SIDE	1	21
M3-4	WEST	200' N. OF INT. C.T.H. "A" & 4th ST. EAST SIDE		21
M1-4	U.S.H. 8	200' N. OF INT. C.T.H. "A" & 4th ST. EAST SIDE		21
M4-8	DETOUR	N.E. QUAD INT. OF C.T.H. "A" & 4th ST. NORTH SIDE	1	21
M3-4	WEST	N.E. QUAD INT. OF C.T.H. "A" & 4th ST. NORTH SIDE		21
M1-4	U.S.H. 8	N.E. QUAD INT. OF C.T.H. "A" & 4th ST. NORTH SIDE		21
M6-1	DIRECTION MARKER	N.E. QUAD INT. OF C.T.H. "A" & 4th ST. NORTH SIDE		21
M4-8	DETOUR	200' E. OF INT. C.T.H. "A" & 4th ST. SOUTH SIDE	1	21
M3-2	EAST	200' E. OF INT. C.T.H. "A" & 4th ST. SOUTH SIDE		21
M1-4	U.S.H. 8	200' E. OF INT. C.T.H. "A" & 4th ST. SOUTH SIDE		21
PRENTICE	WITH ARROW RT.	500' E. OF INT. C.T.H. "A" & 4th ST. NORTH SIDE	1	21
M4-8	DETOUR	1,000' E. OF INT. C.T.H. "A" & 4th ST. NORTH SIDE	1	21
M3-4	WEST	1,000' E. OF INT. C.T.H. "A" & 4th ST. NORTH SIDE		21
M1-4	U.S.H. 8	1,000' E. OF INT. C.T.H. "A" & 4th ST. NORTH SIDE		21
M5-1R	ADVANCE TURN RT.	1,000' E. OF INT. C.T.H. "A" & 4th ST. NORTH SIDE		21

(CONT'D.)

- * REQUIRES 2-24" ORANGE FLAGS ON SIGN
- ◇ SIGN COVERS INCIDENTAL TO TRAFFIC CONTROL ITEMS
- MODIFY TO 2 MILES

(CONT'D.)

SIGNS, TYPE II, REFLECTIVE, TEMPORARY AND SIGN POSTS

DETOUR

SIGN CODE	DESCRIPTION	LOCATION	POSTS 4"x4"x12'	DAYS
M4-8	DETOUR	1,000' E. OF INT. C.T.H. "A" & 4th ST. SOUTH SIDE	1	21
M3-4	WEST	1,000' E. OF INT. C.T.H. "A" & 4th ST. SOUTH SIDE		21
M1-4	U.S.H. 8	1,000' E. OF INT. C.T.H. "A" & 4th ST. SOUTH SIDE		21
M5-1R	ADVANCE TURN RT. WITH ARROW LT.	1,000' E. OF INT. C.T.H. "A" & 4th ST. SOUTH SIDE		21
RHINELANDER		600' W. OF INT. U.S.H. 51 & C.T.H. "A" SOUTH SIDE	EXT. POST	21
M4-8	DETOUR	1,000' W. OF INT. U.S.H. 51 & C.T.H. "A" SOUTH SIDE	1	21
M3-4	WEST	1,000' W. OF INT. U.S.H. 51 & C.T.H. "A" SOUTH SIDE		21
M1-4	U.S.H. 8	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" SOUTH SIDE		21
M5-1L	ADVANCE TURN LT.	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" SOUTH SIDE		21
M4-8	DETOUR	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE	1	21
M3-4	WEST	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE		21
M1-4	U.S.H. 8	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE		21
M5-1L	ADVANCE TURN LT.	1,000' W. OF INT. U.S.H. 8 & C.T.H. "A" NORTH SIDE		21
M4-8	DETOUR	S.W. QUAD INT OF U.S.H. 51 & C.T.H. "A" SOUTH SIDE	EXT. POST	21
M3-2	EAST	S.W. QUAD INT OF U.S.H. 51 & C.T.H. "A" SOUTH SIDE		21
M1-4	U.S.H. 8	S.W. QUAD INT OF U.S.H. 51 & C.T.H. "A" SOUTH SIDE		21
M6-1	DIRECTION MARKER	S.W. QUAD INT OF U.S.H. 51 & C.T.H. "A" SOUTH SIDE		21
M4-8	DETOUR	S.E. QUAD INT. OF U.S.H. 51 & C.T.H. "A" EAST SIDE	1	21
M3-4	WEST	S.E. QUAD INT. OF U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M1-4	U.S.H. 8	S.E. QUAD INT. OF U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M6-1	DIRECTION MARKER	S.E. QUAD INT. OF U.S.H. 51 & C.T.H. "A" EAST SIDE		21
* W20-3	DETOUR 3/4 MI.	3/4 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE	1	21
M3-4	WEST	3/4 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M1-4	U.S.H. 8	3/4 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
* W20-2	DETOUR 1/2 MI.	1/2 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE	1	21
M3-4	WEST	1/2 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M1-4	U.S.H. 8	1/2 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
* W20-2	DETOUR 1/2 MI.	1/2 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE	1	21
M3-4	WEST	1/2 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M1-4	U.S.H. 8	1/2 MILE S. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M4-8	DETOUR	700' S. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE	1	21
M3-4	WEST	700' S. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M1-4	U.S.H. 8	700' S. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M5-1L	ADVANCE TURN LT.	700' S. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M4-8	DETOUR	700' S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE	EXT. POST	21
M3-4	WEST	700' S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M1-4	U.S.H. 8	700' S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M5-1L	ADVANCE TURN LT. WITH ARROW LT.	700' S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
PRENTICE		300' S. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE	EXT. POST	21
M4-8	DETOUR	N.W. OF INT. U.S.H. 51 & C.T.H. "A" NORTH SIDE	EXT. POST	21
M3-4	WEST	N.W. OF INT. U.S.H. 51 & C.T.H. "A" NORTH SIDE		21
M1-4	U.S.H. 8	N.W. OF INT. U.S.H. 51 & C.T.H. "A" NORTH SIDE		21
M4-8	DETOUR	100' N. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE	1	21
M3-4	WEST	100' N. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M1-4	U.S.H. 8	100' N. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M6-1	DIRECTION MARKER	100' N. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
PRENTICE	WITH ARROW RT.	200' N. OF INT. U.S.H. 8 & C.T.H. "A" WEST SIDE	EXT. POST	21
M4-8	DETOUR	100' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE	EXT. POST	21
M3-2	EAST	100' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M1-4	U.S.H. 8	100' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M4-8	DETOUR	100' E. OF INT. U.S.H. 51 & C.T.H. "A" NORTH SIDE	EXT. POST	21
M3-2	EAST	100' E. OF INT. U.S.H. 51 & C.T.H. "A" NORTH SIDE		21
M1-4	U.S.H. 8	100' E. OF INT. U.S.H. 51 & C.T.H. "A" NORTH SIDE		21
M6-3	DIRECTION MARKER	100' E. OF INT. U.S.H. 51 & C.T.H. "A" NORTH SIDE		21
M4-8	DETOUR	100' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE	EXT. POST	21
M3-2	EAST	100' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M6-3	DIRECTION MARKER	100' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M4-8	DETOUR	500' N. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE	EXT. POST	21
M3-4	WEST	500' N. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M1-4	U.S.H. 8	500' N. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M5-1R	ADVANCE TURN RT.	500' N. OF INT. U.S.H. 51 & C.T.H. "A" WEST SIDE		21
M4-8	DETOUR	500' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE	1	21
M3-4	WEST	500' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M1-4	U.S.H. 8	500' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21
M5-1R	ADVANCE TURN RT.	500' N. OF INT. U.S.H. 51 & C.T.H. "A" EAST SIDE		21

(CONT'D.)

* REQUIRES 2-24" ORANGE FLAGS ON SIGN