

9852-04-70

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Sheet No.	Typical Sections and Details
Sheet No.	Estimate of Quantities
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Sheet No.	Sign Plates
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TOTAL SHEETS =



Design Designation

A.D.T. (1995)	=	20
A.D.T. (2015)	=	30
D.H.V. (2015)	=	3
D.	=	60/40
T.	=	10.0%
DESIGN SPEED	=	40 mph
ESALS	=	N/A

Conventional Symbols

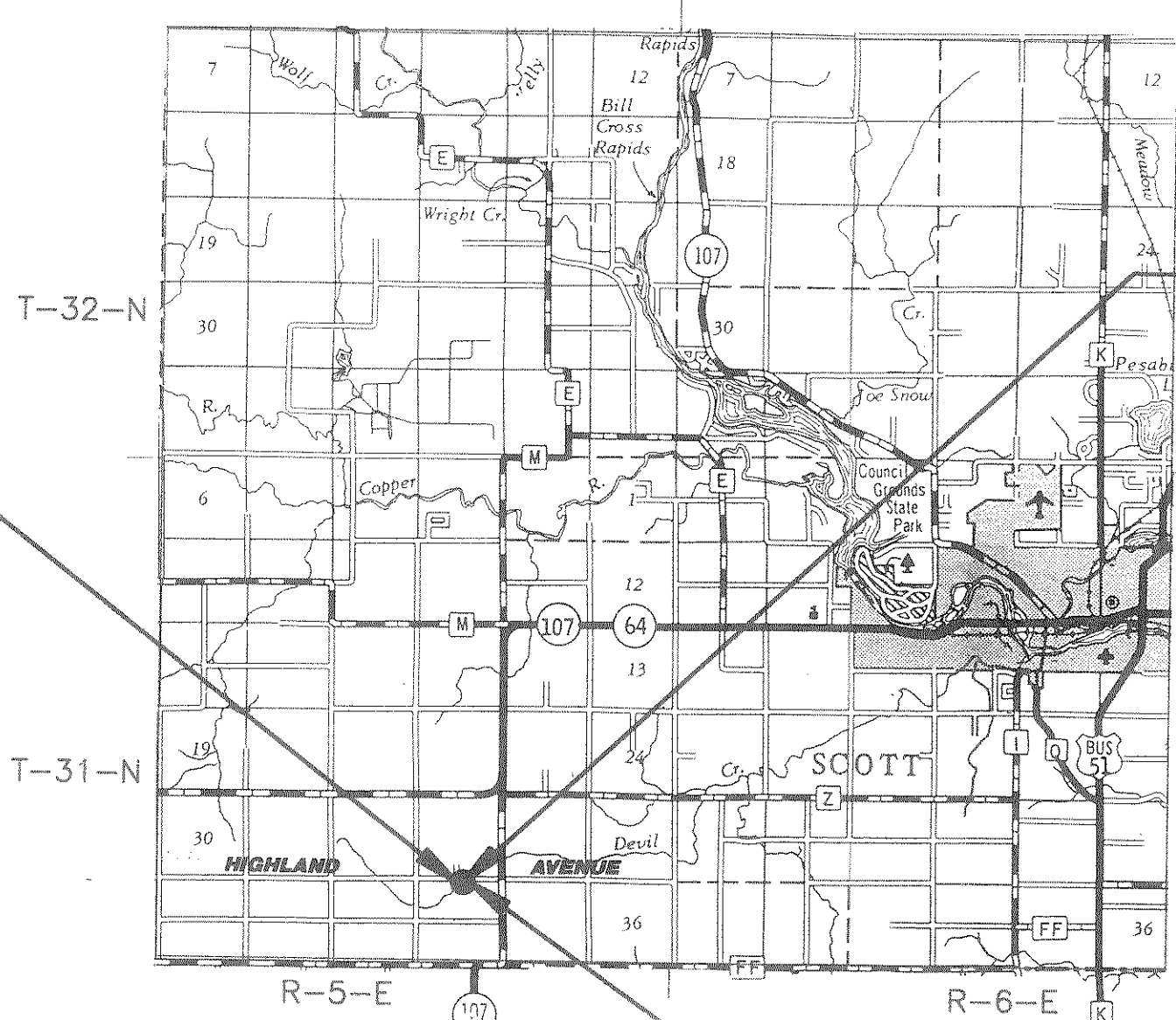
County Line	.....	Combustible Fluids	.....
Town or Range Line	.....	Railroad	.....
Section Line	.....	Fence	.....
Corporate Limits	.....	Culvert (in Place)	.....
Property Line	.....	Culvert (Required)	.....
Lot Line	.....	Power Pole	.....
Existing Right of Way Line	.....	Telephone Pole	.....
New Right of Way Line	.....	Right of Way Monument (Type)	.....
Reference Line	.....	Marsh	.....
Slope Intercept	.....	Wooded or Shrub Area	.....
Existing Roadway or	.....	Grade Line Elevation	.....
Private Entrance	.....	Water	.....
Limited Easement	.....	Gas	.....
Right of Way Point	.....	Telephone	.....
Silt Fence	.....	Electric	.....
Erosion Bales	.....	Cable Television	.....
Sod	.....	Fiber Optic	.....
Ditch Dike	.....	Sanitary Sewer	.....
Reinforced Sod	.....	Storm Sewer	.....
Intercepting Embankment	.....	
Riprap	.....	
Erosion Mat	.....	

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

**PLAN OF PROPOSED IMPROVEMENT
DEVILS CREEK BRIDGE & APPROACHES**

(HIGHLAND AVENUE)
TOWN OF CORNING
TOWN ROAD
LINCOLN COUNTY

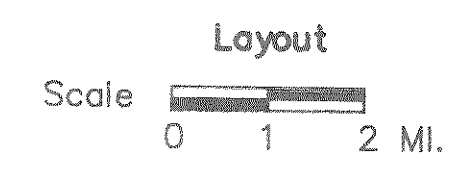
STATE PROJECT NUMBER
9852-04-70



BEGIN PROJECT
STA. 9+00
X: 2,039,500 (± 100')
Y: 474,500 (± 100')

END PROJECT
STA. 11+00

STRUCTURE
B-35-131



Total Net Length of Centerline: 0.038 Mi. (Rural)

COORDINATES ARE SCALED FROM THE U.S.G.S. TOPOGRAPHIC MAP ALEXANDER LAKE, WISCONSIN QUADRANGLE, FOR IDENTIFICATION ONLY.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
9852-04-70		

ACCEPTED FOR
COUNTY OF LINCOLN

DATE _____ COMMISSIONER _____

ORIGINAL PLANS PREPARED BY:
MSA MID-STATE ASSOCIATES, INC.
1230 South Blvd, Baraboo, WI 53913

Date: January 17, 1994 Signature: Marvin S. Ruhland

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

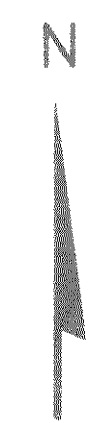
PREPARED BY

Surveyor	MID-STATE ASSOCIATES, INC.
Designer	MID-STATE ASSOCIATES, INC.
District Examiner	DANIEL T. KUHN
District Supervisor	ALLAN J. PETERSON
Proj. Dev. Engineer	
C.O. Examiner	

APPROVED FOR DISTRICT OFFICE

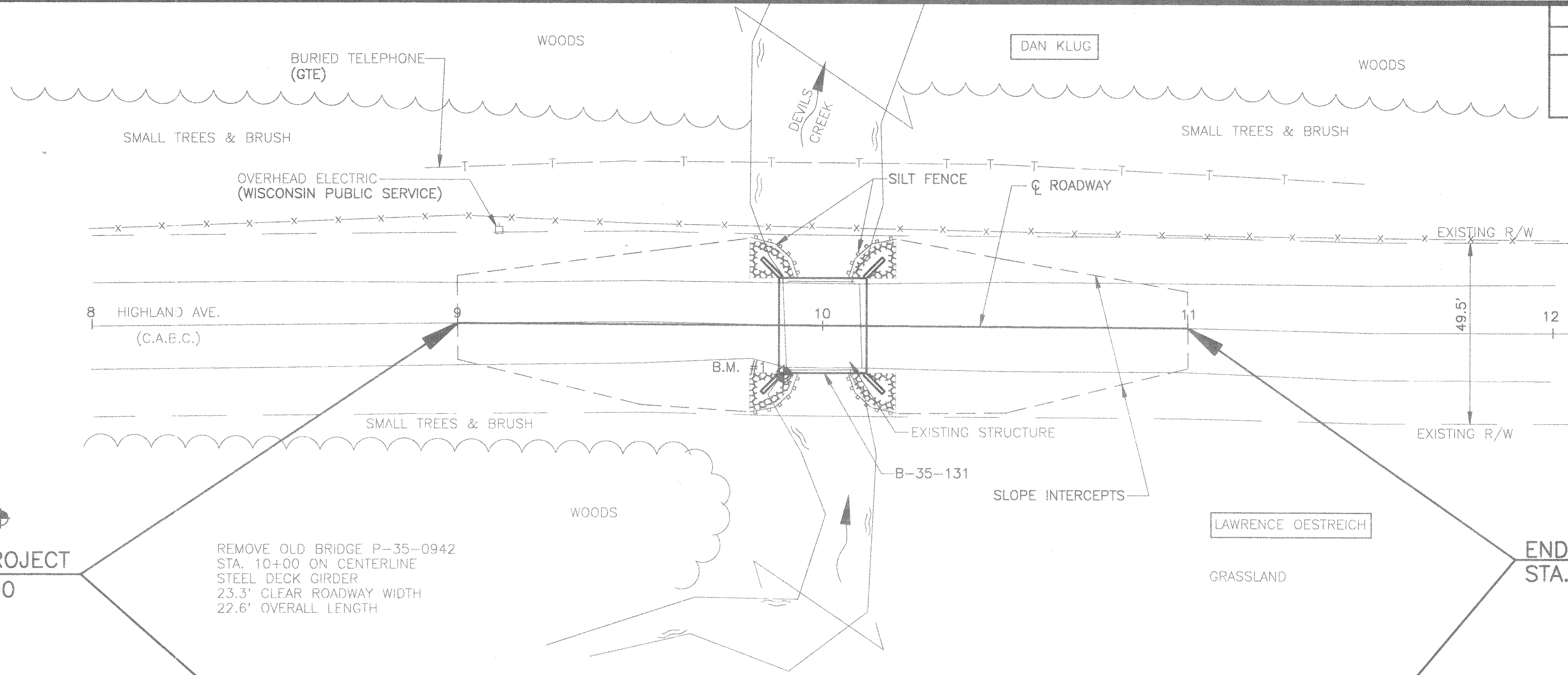
DATE: _____ (Signature) _____

BENCHMARKS			
NO.	STATION	DESCRIPTION	ELEVATION
1	9+88, 13' RT.	TOP OF S.W. WINGWALL	1357.87
2	7+75, 53' RT.	TOP OF BOULDER	1358.05

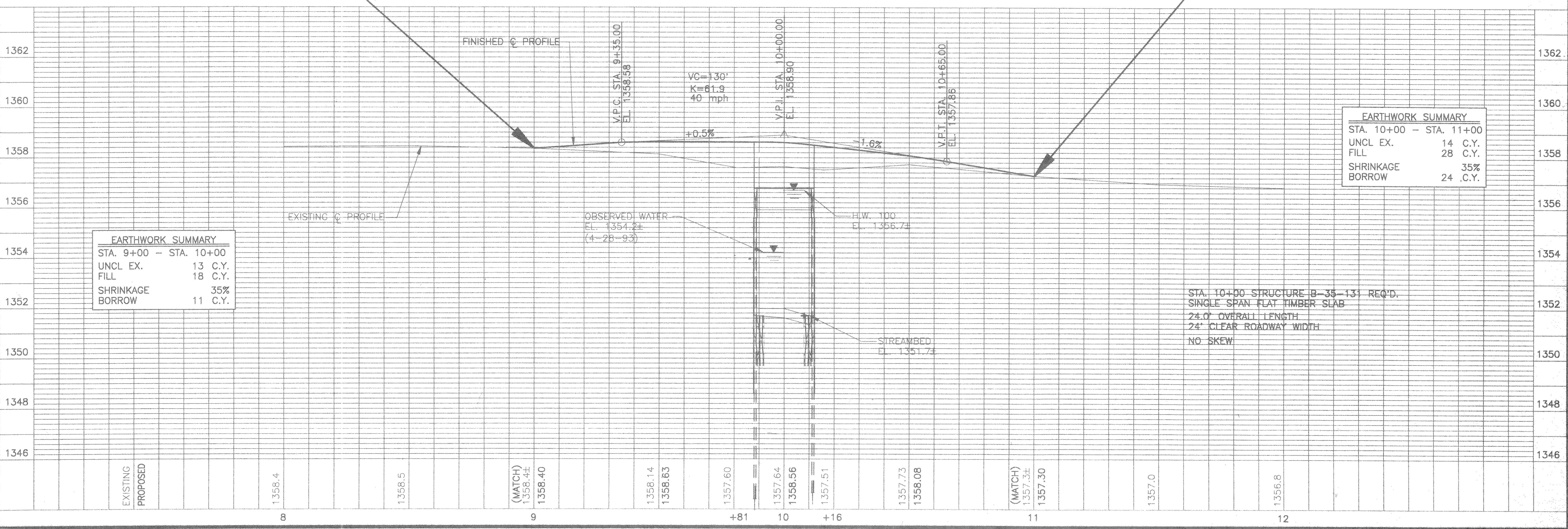


BEGIN PROJECT
STA. 9+00

END PROJECT
STA. 11+00



REMOVE OLD BRIDGE P-35-0942
STA. 10+00 ON CENTERLINE
STEEL DECK GIRDER
23.3' CLEAR ROADWAY WIDTH
22.6' OVERALL LENGTH

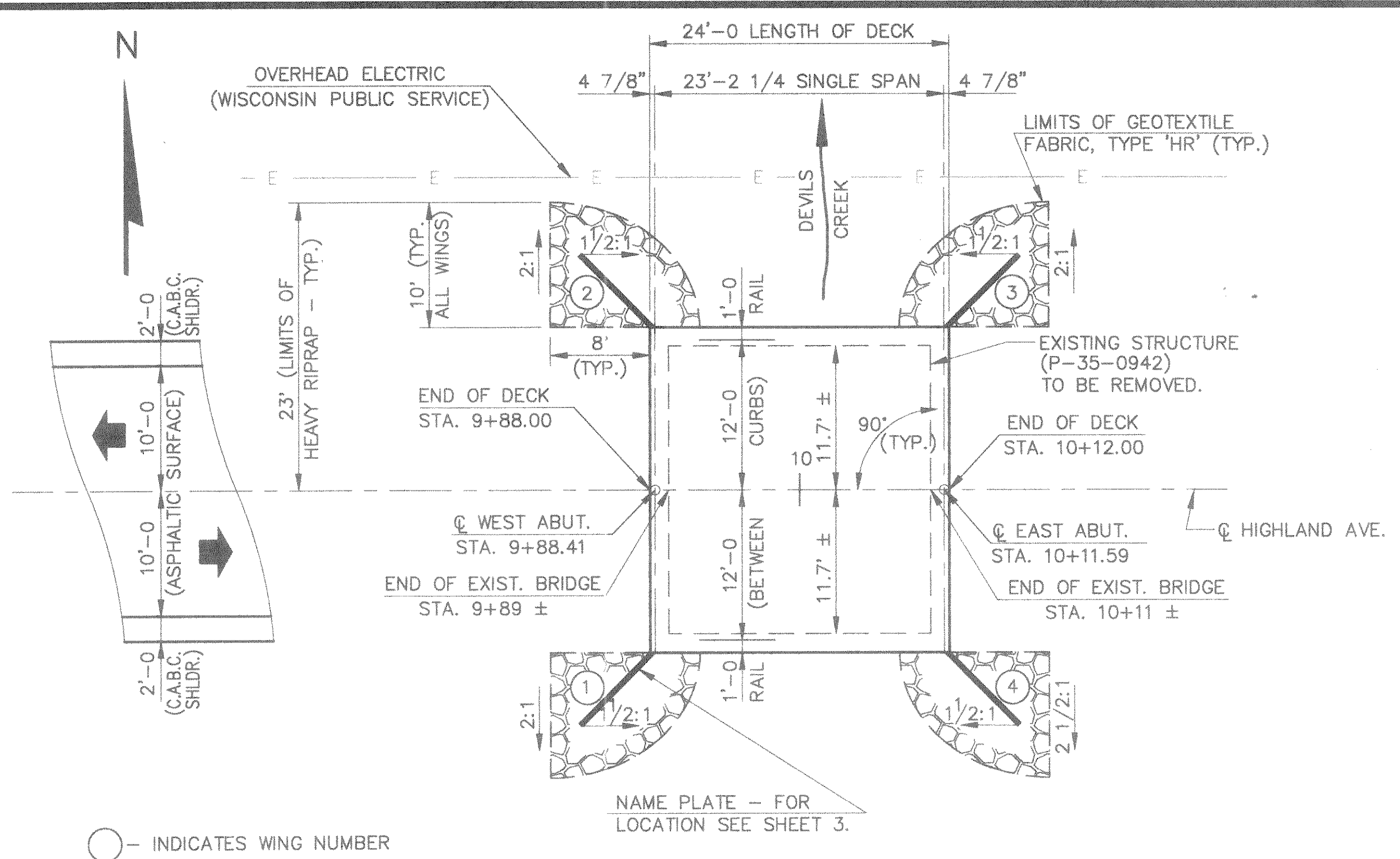


EARTHWORK SUMMARY	
STA. 9+00 - STA. 10+00	
UNCL EX.	13 C.Y.
FILL	18 C.Y.
SHRINKAGE	35%
BORROW	11 C.Y.

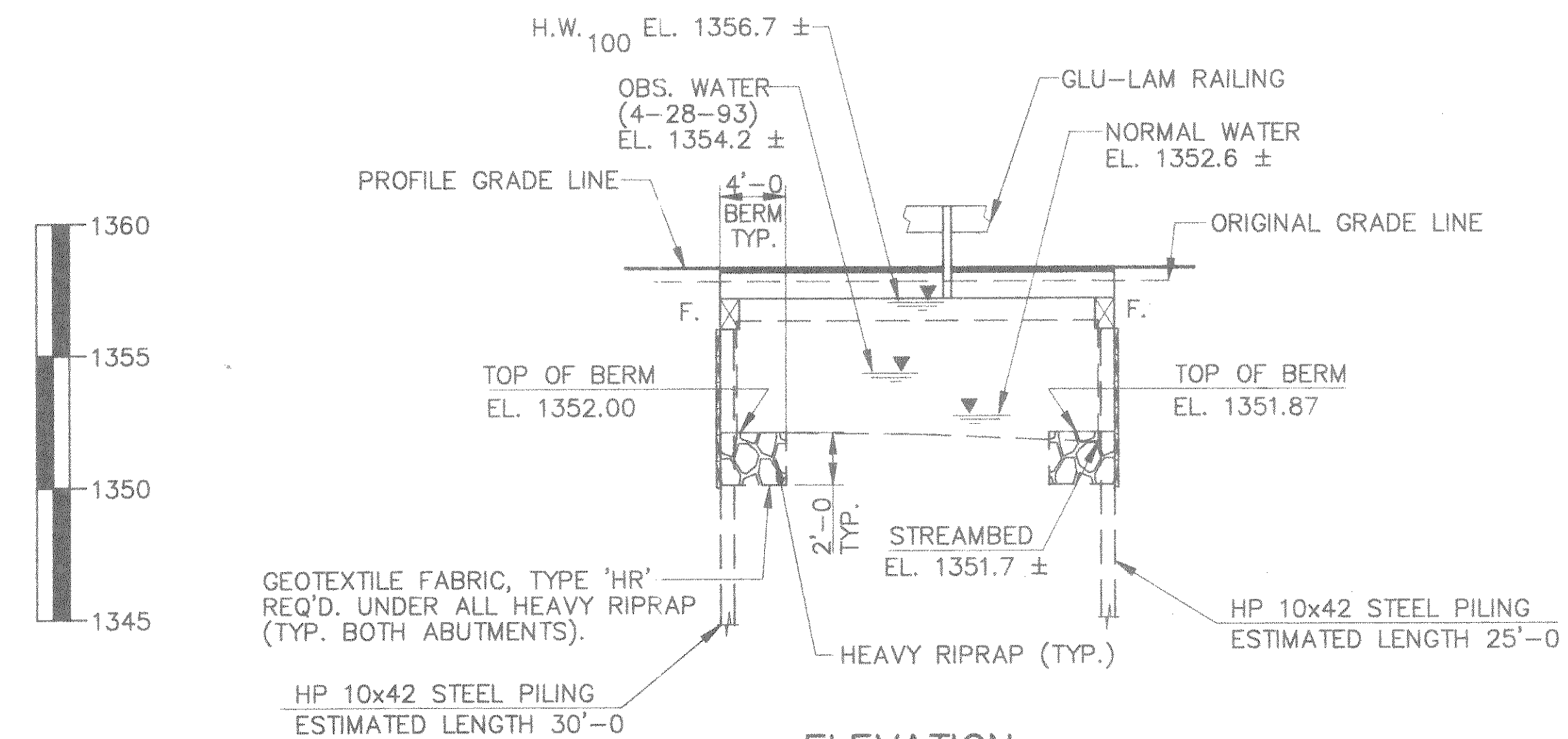
EARTHWORK SUMMARY	
STA. 10+00 - STA. 11+00	
UNCL EX.	14 C.Y.
FILL	28 C.Y.
SHRINKAGE	35%
BORROW	24 C.Y.

STA. 10+00 STRUCTURE B-35-131 REQ'D.
SINGLE SPAN FLAT TIMBER SLAB
24' OVERALL LENGTH
24' CLEAR ROADWAY WIDTH
NO SKEW

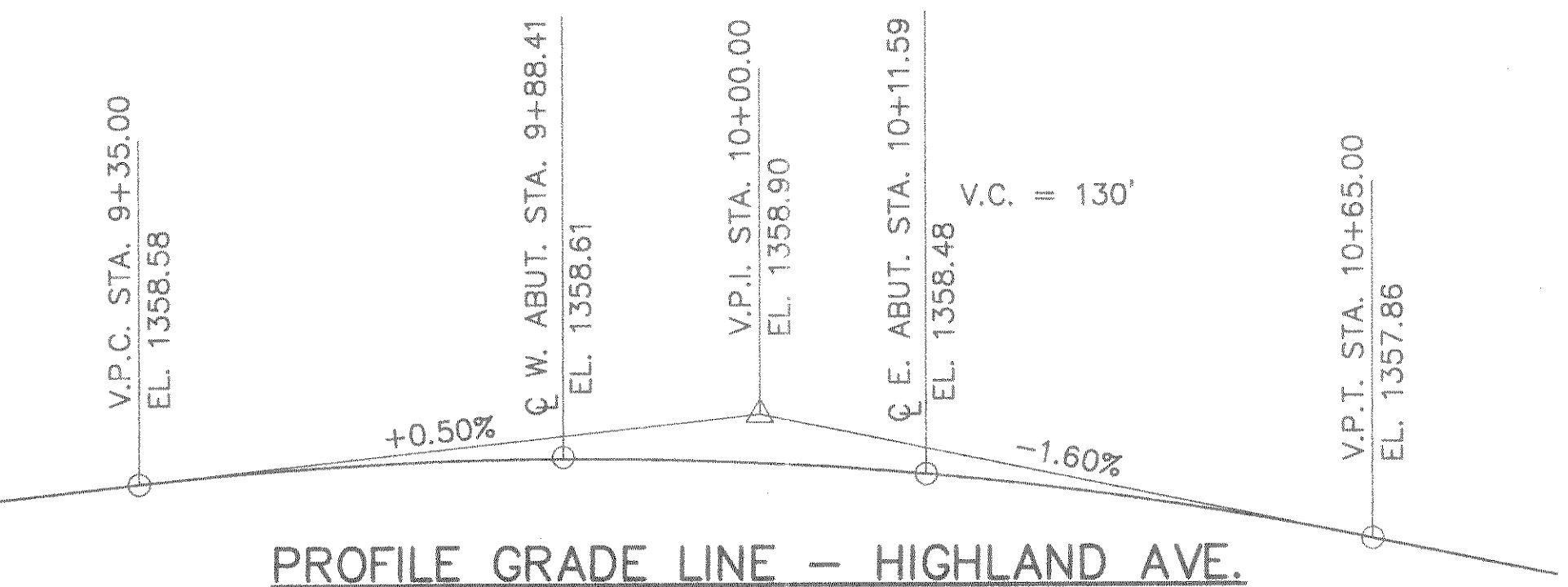
BENCHMARKS			
NO.	LOCATION	DESCRIPTION	ELEV.
1	9+88, 13' RT.	TOP OF S.W. WINGWALL	1357.87
2	7+75, 53' RT.	TOP OF BOULDER	1358.05



PLAN
(SINGLE SPAN TIMBER SLAB)



ELEVATION
(NORMAL TO DEVILS CREEK)



GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

CONSTRUCTION REQUIREMENTS SHALL CONFORM TO SECTION 507.

ALL TIMBER IS TO BE CREOSOTE OR PENTACHLOROPHENOL TREATED PER SECTION 507.

ALL HARDWARE TO BE GALVANIZED PER SECTION 507.

ALL TIMBER IS ROUGH CUT UNLESS OTHERWISE NOTED.

TIMBER CONNECTORS AND HARDWARE SHALL BE INCLUDED IN THE BID ITEM "TREATED LUMBER AND TIMBER".

DESTROY THREADS ON ALL BOLTS WITH A CENTER PUNCH AFTER TIGHTENING NUT.

BOLT PROJECTIONS EXCEEDING 1" SHALL BE CUT OFF. REPAIR END OF BOLT BY PAINTING WITH ZINC RICH PRIMER.

GLU-LAM RAILING & TIMBER DECK PANELS TO BE INCLUDED IN THE BID ITEM, "TREATED LUMBER AND TIMBER".

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE FABRIC TO THE EXTENT SHOWN ON THIS SHEET AND IN THE ABUTMENT DETAILS.

THIS STRUCTURE WILL REPLACE EXISTING BRIDGE P-35-0942, A 22.6 FT. LONG SINGLE SPAN, 23.3 FT. CLEAR ROADWAY WIDTH, STEEL DECK GIRDER SET ON FULL RETAINING CONCRETE ABUTMENTS.

THE EXISTING GROUNDLINE SHALL BE THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES" FOR THE ABUTMENTS.

AT THE ABUTMENTS ALL EXCAVATED VOLUME NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH GRANULAR BACKFILL. PAYMENT WILL BE INCLUDED IN THE BID ITEM "EXCAVATION FOR STRUCTURES, BRIDGES B-35-131".

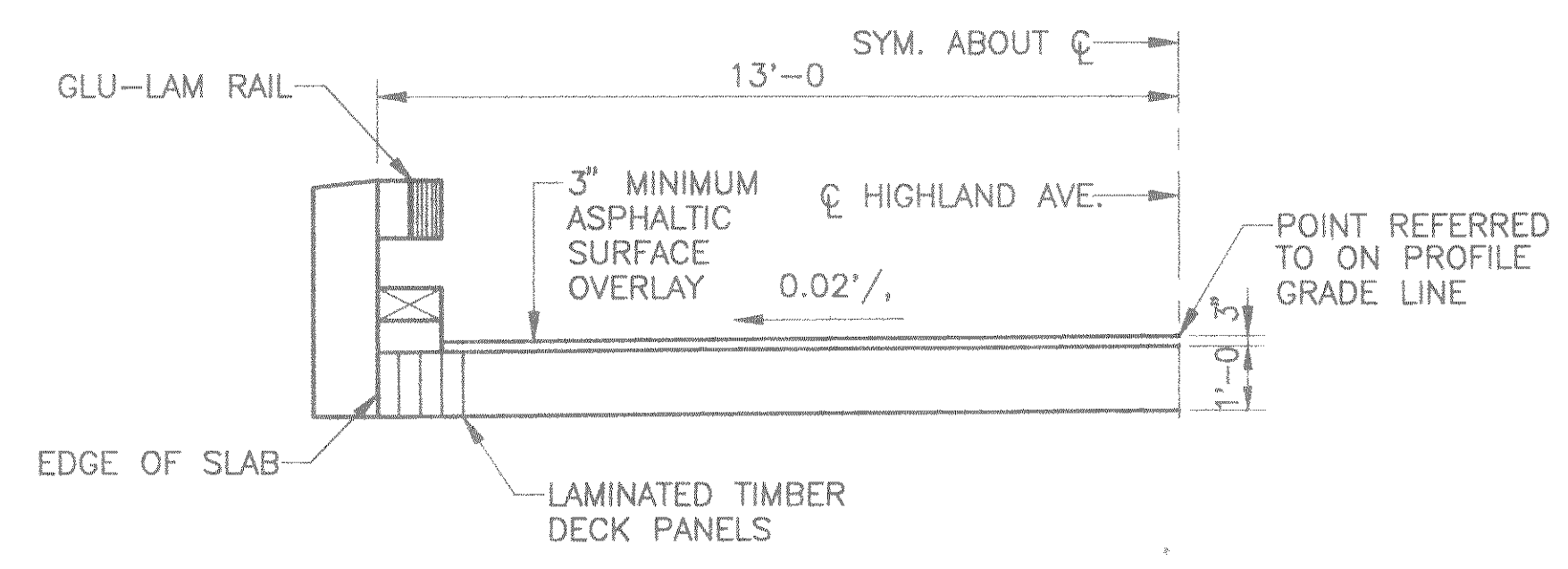
DEWATERING IF NECESSARY TO ATTACH THE BOTTOM TIMBER BACKING PLANKS FOR THE ABUTMENTS SHALL BE INCLUDED IN THE BID ITEM "EXCAVATION FOR STRUCTURES, BRIDGES B-35-131".

PILES AND STEEL PLATES SHALL BE PAINTED PER SUPPLEMENTAL SPECIFICATIONS 511.3.5.

SUPERSTRUCTURE SHALL BE IN PLACE BEFORE ABUTMENTS ARE BACKFILLED ABOVE THE BERM ELEVATIONS. BACKFILL SIMULTANEOUSLY TO AVOID RACKING OF SUPERSTRUCTURE.

GENERAL NOTE FOR FUTURE MAINTENANCE

WHEN PLACING FUTURE WEARING SURFACE OVERLAY ON TOP OF ASPHALTIC SURFACE SHOWN ON THIS PLAN, THE THICKNESS OF THE OVERLAY MUST BE TAPERED NEAR THE VICINITY OF THE RAILING TO MAINTAIN THE REQUIRED (CRASH TESTED) DISTANCE OF 32 INCHES FROM TOP OF WEARING SURFACE TO TOP OF RAIL.



HALF-CROSS SECTION THRU ROADWAY

TOTAL ESTIMATED QUANTITIES

BID ITEMS	UNIT	WEST ABUT.	EAST ABUT.	SUPER	TOTAL
REMOVING OLD BRIDGE, STATION 10+00	L.S.				1
EXCAVATION FOR STRUCTURES, BRIDGES B-35-131	L.S.				1
STRUCTURAL CARBON STEEL	LB.	160	160		320
TREATED LUMBER AND TIMBER	MBM	1.8	1.8	8.9	12.5
STEEL PILING DELIVERED & DRIVEN HP 10 INCH, 42 POUND	L.F.	210	175		385
HEAVY RIPRAP	C.Y.	25	25		50
GEOTEXTILE FABRIC, TYPE DF	S.Y.	35	35		70
GEOTEXTILE FABRIC, TYPE HR	S.Y.	50	50		100

DESIGN DATA

LIVELOAD: DESIGN RATING : HS-20
INVENTORY RATING : HS-25
OPERATING RATING : HS-35

TRAFFIC DATA: A.D.T. (1995) = 20
A.D.T. (2015) = 30

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.
MAX. STD. PERMIT VEHICLE LOAD = 180 KIPS

ALLOWABLE DESIGN STRESSES:

TIMBER RAIL POSTS (NO HEARTWOOD ALLOWED) fb = 1,900 P.S.I.
GLU-LAM RAILING (DRY CONDITION) fb = 2,400 P.S.I.
FLOOR PLANKS FOR PREFAB PANELS fb = 1,500 P.S.I.
ALL OTHER TIMBER fb = 1,300 P.S.I.

FOUNDATION DATA:

ABUTMENTS SHALL BE SUPPORTED ON HP 10x42 STEEL PILING DRIVEN TO SOUND ROCK WITH A MINIMUM BEARING VALUE OF 40 TONS PER PILE. ESTIMATED PILE LENGTHS ARE 30'-0" AT THE WEST ABUTMENT AND 25'-0" AT THE EAST ABUTMENT.

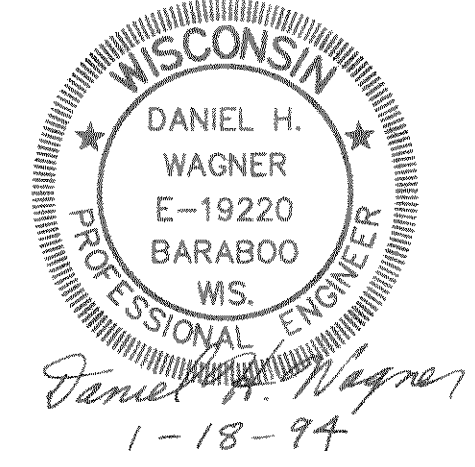
HYDRAULIC DATA:

100 YEAR FREQUENCY

DRAINAGE AREA 5.36 SQ. MI.
Q₁₀₀ 510 C.F.S.
VELOCITY 8.1 F.P.S.
WATERWAY AREA 63 SQ. FT.
HIGH WATER₁₀₀ ELEVATION 1356.7 ±
ROADWAY OVERFLOW DESIGN FREQUENCY N/A
SCOUR CRITICAL CODE 8

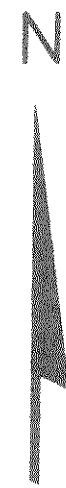
LIST OF DRAWINGS

- GENERAL PLAN
- SUBSURFACE EXPLORATION
- ABUTMENTS
- ABUTMENT DETAILS
- SUPERSTRUCTURE
- SUPERSTRUCTURE DETAILS



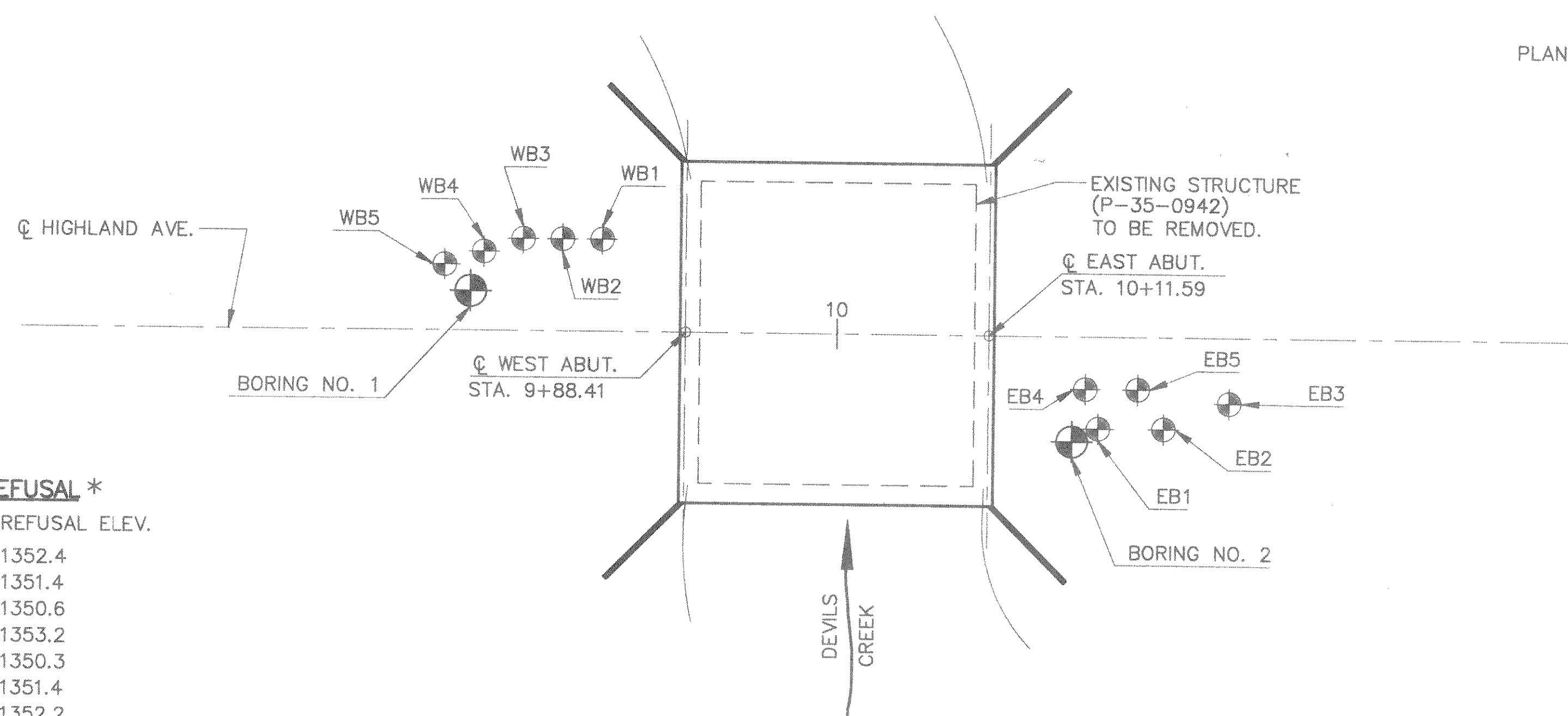
BRIDGE OFFICE CONTACT: (608) 264-9425

No.	Date	Revision	By
PLANS PREPARED BY			
MID-STATE ASSOCIATES, INC. 1230 S. BLVD., BARABOO, WI. 53913			
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-35-131			
HIGHLAND AVENUE OVER DEVILS CREEK			
County	LINCOLN	Town/City/Village	CORNING
Design Spec.	AASHTO 1992	Load	HS-20
Design By	DHW	Design Checked	RJR
Drawn By	RLR	Plans Checked	DHW
Approved _____ State Bridge Engineer Date _____			
GENERAL PLAN			SHEET 1 OF 6



BORINGS PERFORMED BY:
 ENVIRONMENTAL & FOUNDATION DRILLING, INC.
 WAUNAKEE, WISCONSIN
 BORINGS COMPLETED ON: AUGUST 10, 1993

PLANS PREPARED BY:
 MID-STATE ASSOCIATES, INC.
 BARABOO, WISCONSIN



BORINGS MEETING SHALLOW REFUSAL *

BORING	STATION	LOCATION	REFUSAL ELEV.
WB1	9+82	7' LT.	1352.4
WB2	9+79	7' LT.	1351.4
WB3	9+76	7' LT.	1350.6
WB4	9+73	6' LT.	1353.2
WB5	9+70	5' LT.	1350.3
EB1	10+20	7' RT.	1351.4
EB2	10+25	7' RT.	1352.2
EB3	10+30	5' RT.	1351.0
EB4	10+19	4' RT.	1352.6
EB5	10+23	4' RT.	1352.4

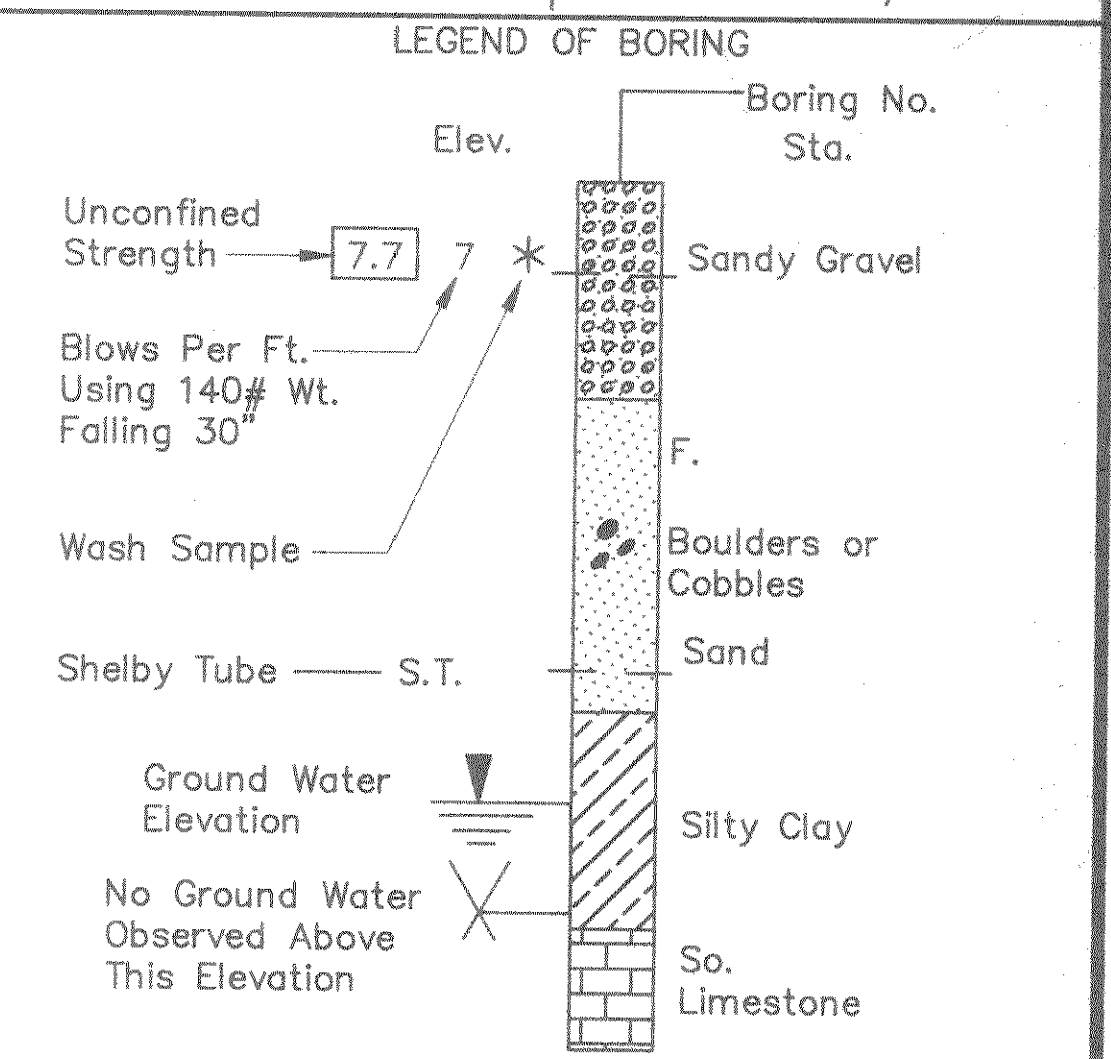
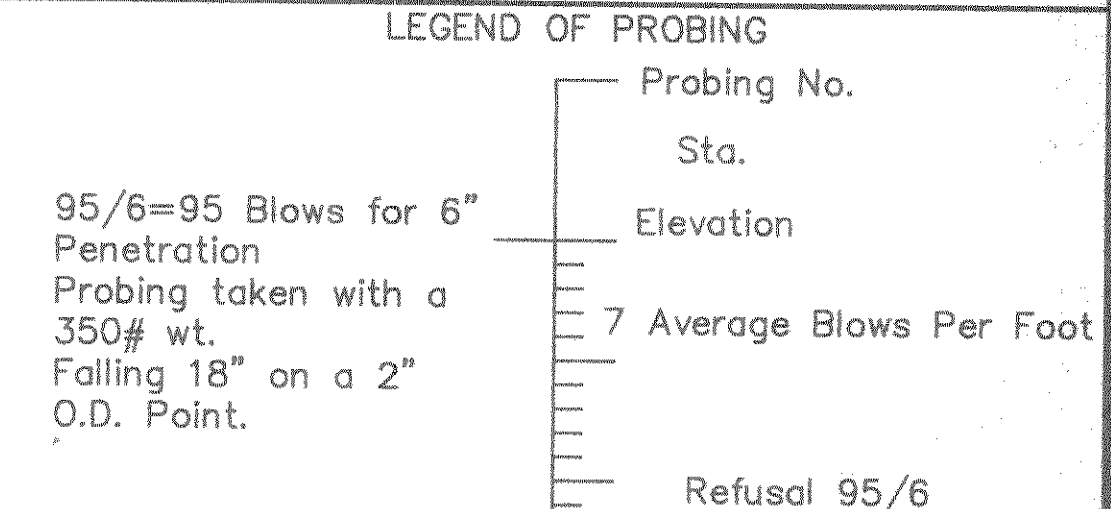
* - REFUSAL ASSUMED TO BE ON BOULDERS

9852-04-71

ABBREVIATIONS
 F - Fine M - Medium C - Coarse
 Ws - Weathered So - Sound

MATERIAL SYMBOLS

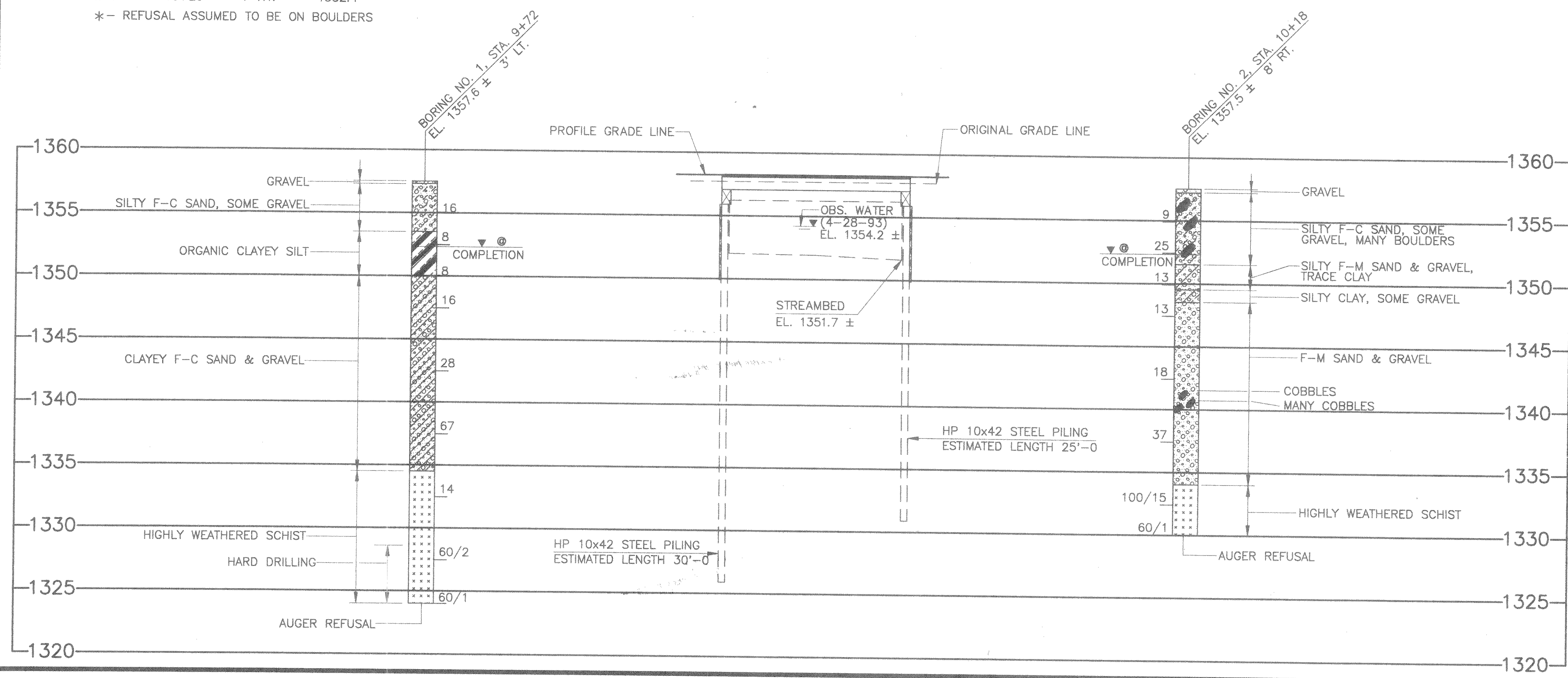
Topsoil	Silt	Sandstone
Sand	Peat	Limestone
Gravel	Clay	Igneous Rock



Unless otherwise specified, the blows per foot at the locations indicated are based on driving a 2" O.D. x 1.4" I.D. split spoon sampler with a 140# hammer having a free fall of 30". The blow count is taken in undisturbed soil immediately below a cased or open hole eliminating side friction on the drive pipe.

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

To obtain relative data concerning the character of material in and upon which the foundation might be built, borings and/or soundings were made at points approximately as indicated on this drawing. The data presented herein represents the findings of the subsurface explorations made. However, because the depths investigated are limited and the area of the borings and/or soundings is very small in relation to the entire area, the Division of Highways does not warrant conditions below the depths investigated or that the classification of material encountered, in these investigations is necessarily typical of the entire site.



No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-35-131			
Const. Spec. WI "89"	Drawn By MMR/RLR	Plans Checked DHW	
SUBSURFACE EXPLORATION			SHEET 2 OF 6

BILL OF HARDWARE (2 ABUTMENTS)		
NO. REQ'D.	ITEM	LOCATION
8	5/8" ϕ x 1'-4 DOME HEAD DRIVE SPIKES	AT CAP BLOCKS
10	3/4" ϕ x 1'-4 DOME HEAD BOLTS	ABUT. CAP VERT.
10	5/8" ϕ x 5" MACHINE BOLTS	BOTTOM ABUT. BACKING PLANKS
20	5/8" ϕ x 9" MACHINE BOLTS	ABUT. CLEATS
6	3/4" ϕ x 2'-4 MACHINE BOLTS	PILE STAY AT CAP
12	3/4" ϕ x 1'-2 MACHINE BOLTS	BOTTOM OF PILE STAY
12	2 5/8" ϕ TOOTHED RING CONNECTORS	ABUT. CLEATS
20	3/8" ϕ x 6" BOAT SPIKES	WING CAP
4	5/8" ϕ x 6" DOME HEAD BOLTS	WING BACKING BOTTOM
* 184	60d NAILS	WING CLEAT
120	40d NAILS	NO. 9 ANCHOR PLATES
60	NO. 9 ANCHOR PLATES	BACKING @ PILES
26	3/8" ϕ RING SHANK SPIKES	TAPERED TIMBER FILLER

BILL OF HARDWARE IS FOR INFORMATION ONLY. ALL REQUIRED CONNECTORS AND HARDWARE FOR TIMBER CONNECTIONS ARE INCLUDED IN COST FOR "TREATED LUMBER AND TIMBER".

ALL LUMBER AND TIMBER SHALL BE TREATED, FULL SAWED, AND ROUGH CUT WITH AN ALLOWABLE BENDING STRESS AS REQUIRED. ALL LUMBER AND TIMBER IN THE ABUTMENTS SHALL BE CREOSOTE OR PENTACHLOROPHENOL TREATED.

* - NAILS NOT INCLUDED FOR TIMBER BACKING SPLICES.

BILL OF STRUCTURAL CARBON STEEL (2 ABUTMENTS)				
NO. REQ'D.	ITEM	LOCATION	LBS.	
10	3/8" x 10 1/4" x 1'-1 PLATES	BODY PILES	142	
20	3/8" x 6" x 7 1/2" PLATES	"	96	
4	3/8" x 9 3/4" x 10 1/4" PLATES	WING	40	
8	3/8" x 6 1/2" x 7 1/2" PLATES	"	42	
TOTAL STRUCTURAL CARBON STEEL			320	

* - PLATES FOR OPTIONAL PILE CAP SPLICE NOT INCLUDED.

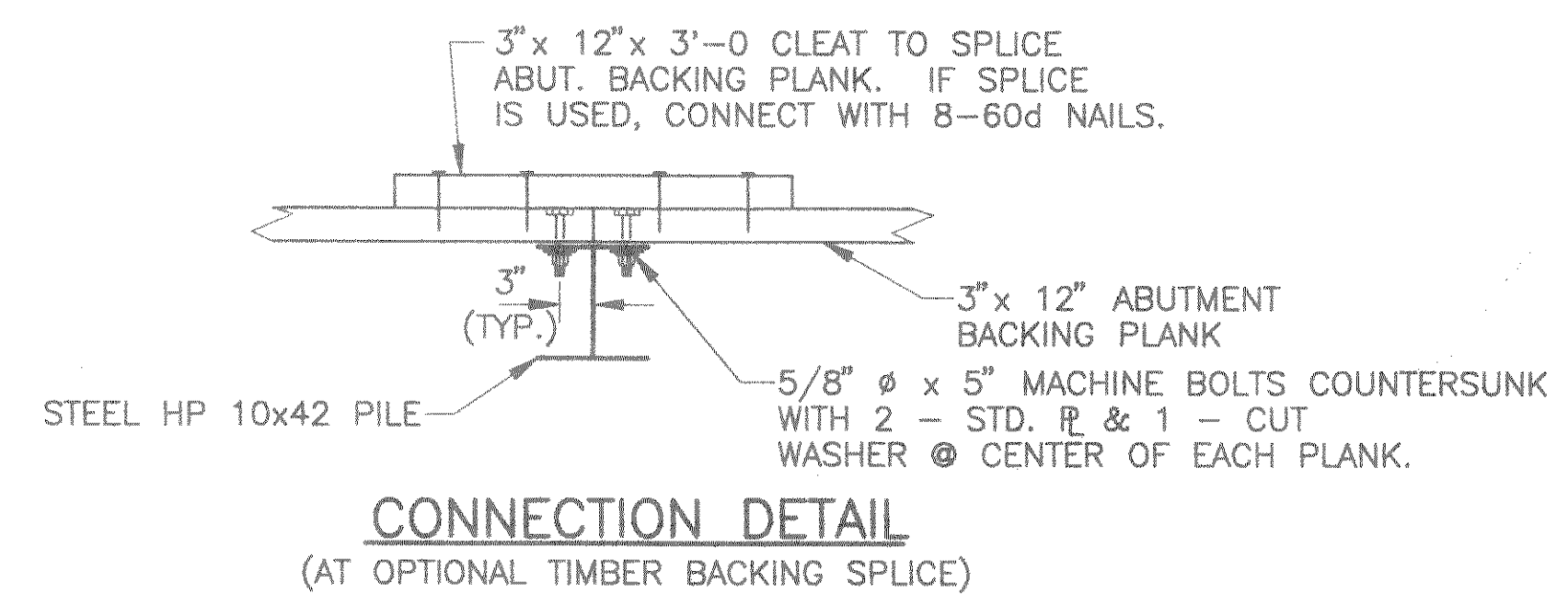
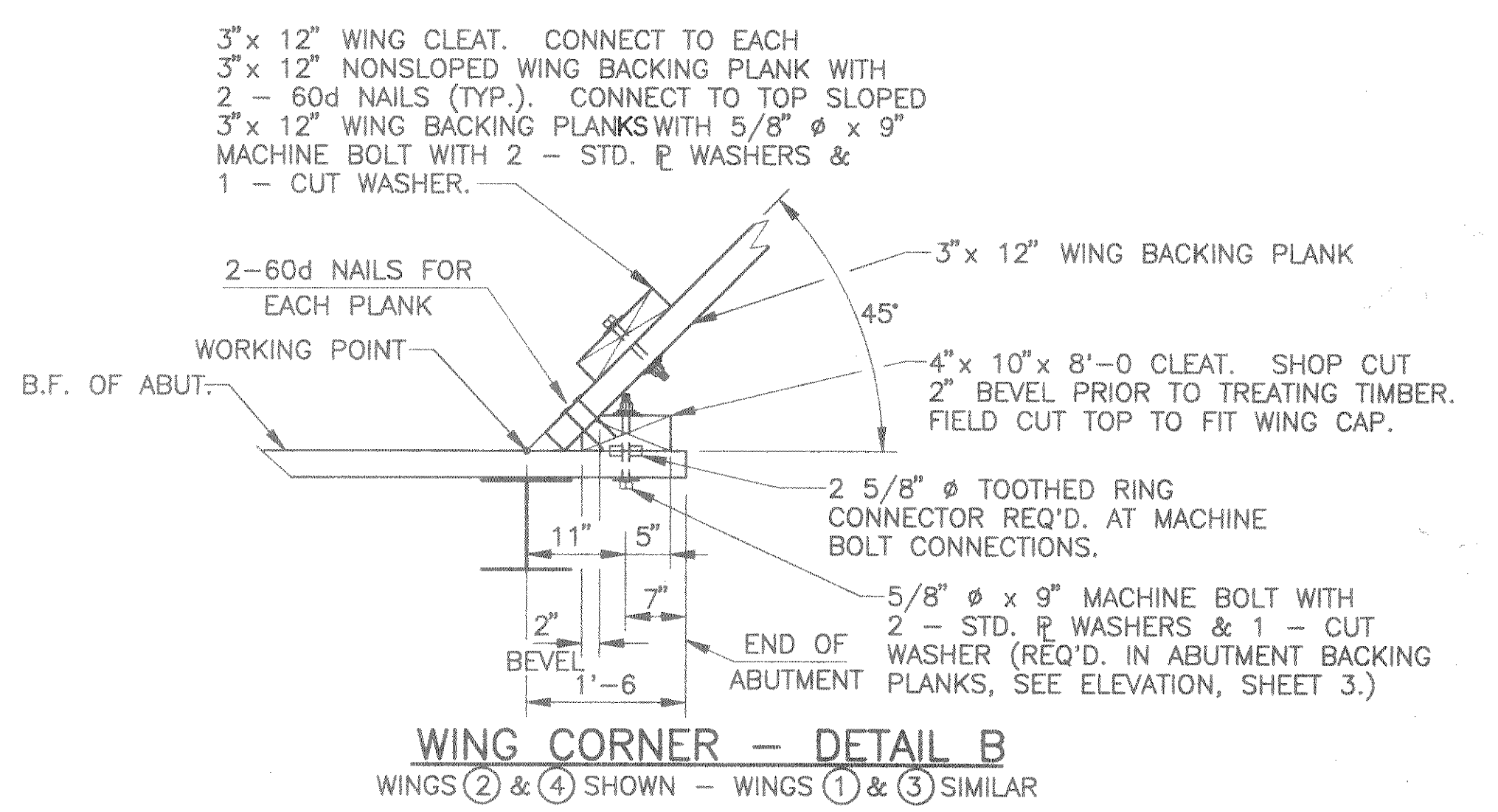
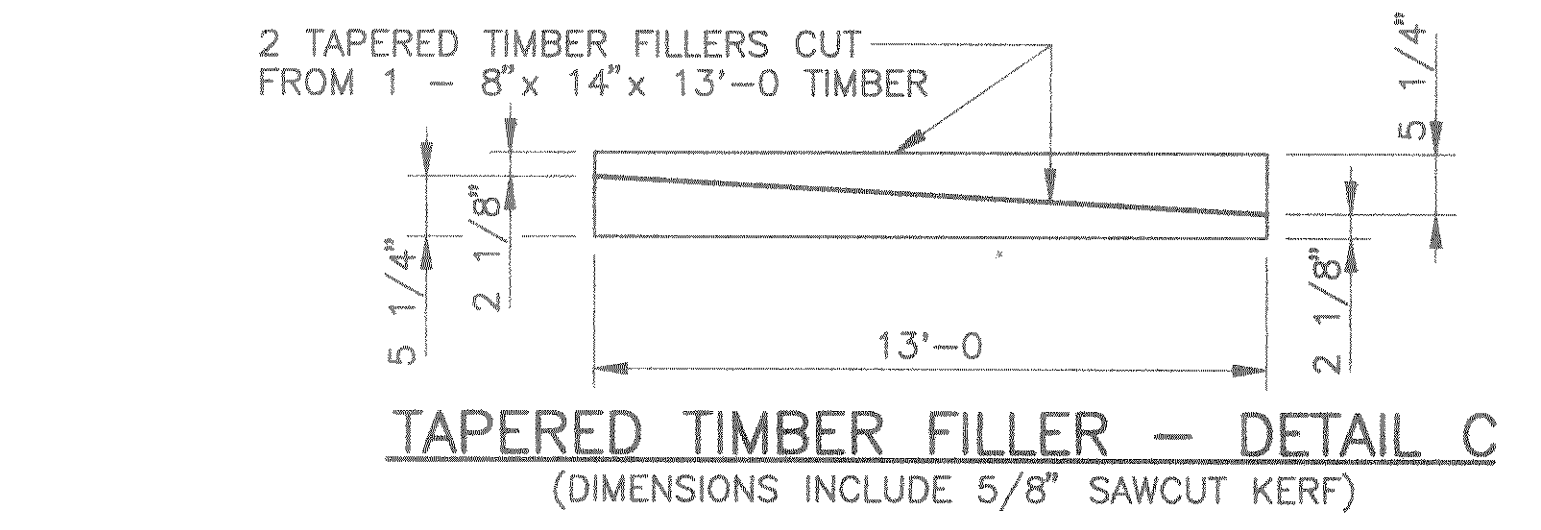
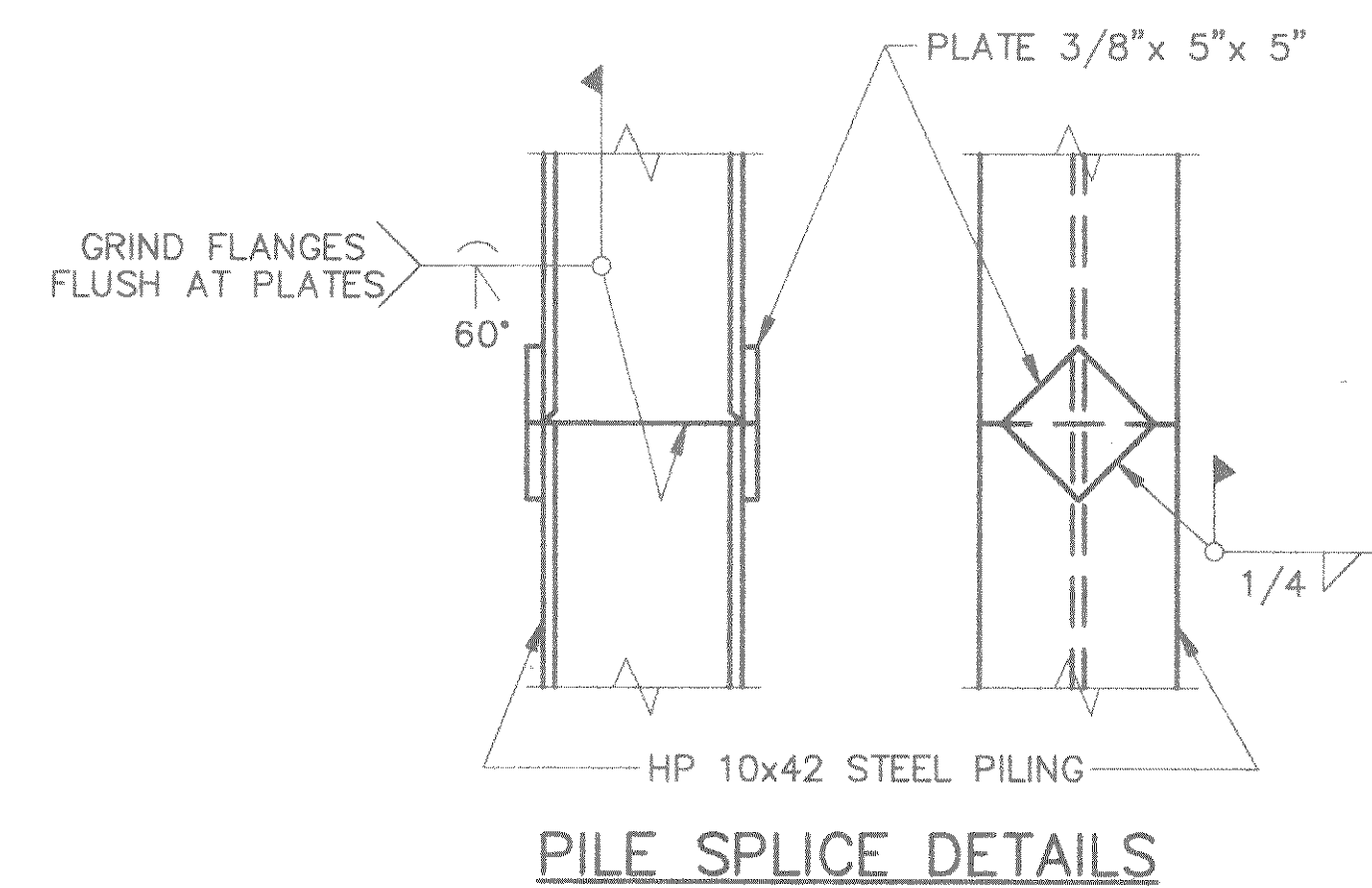
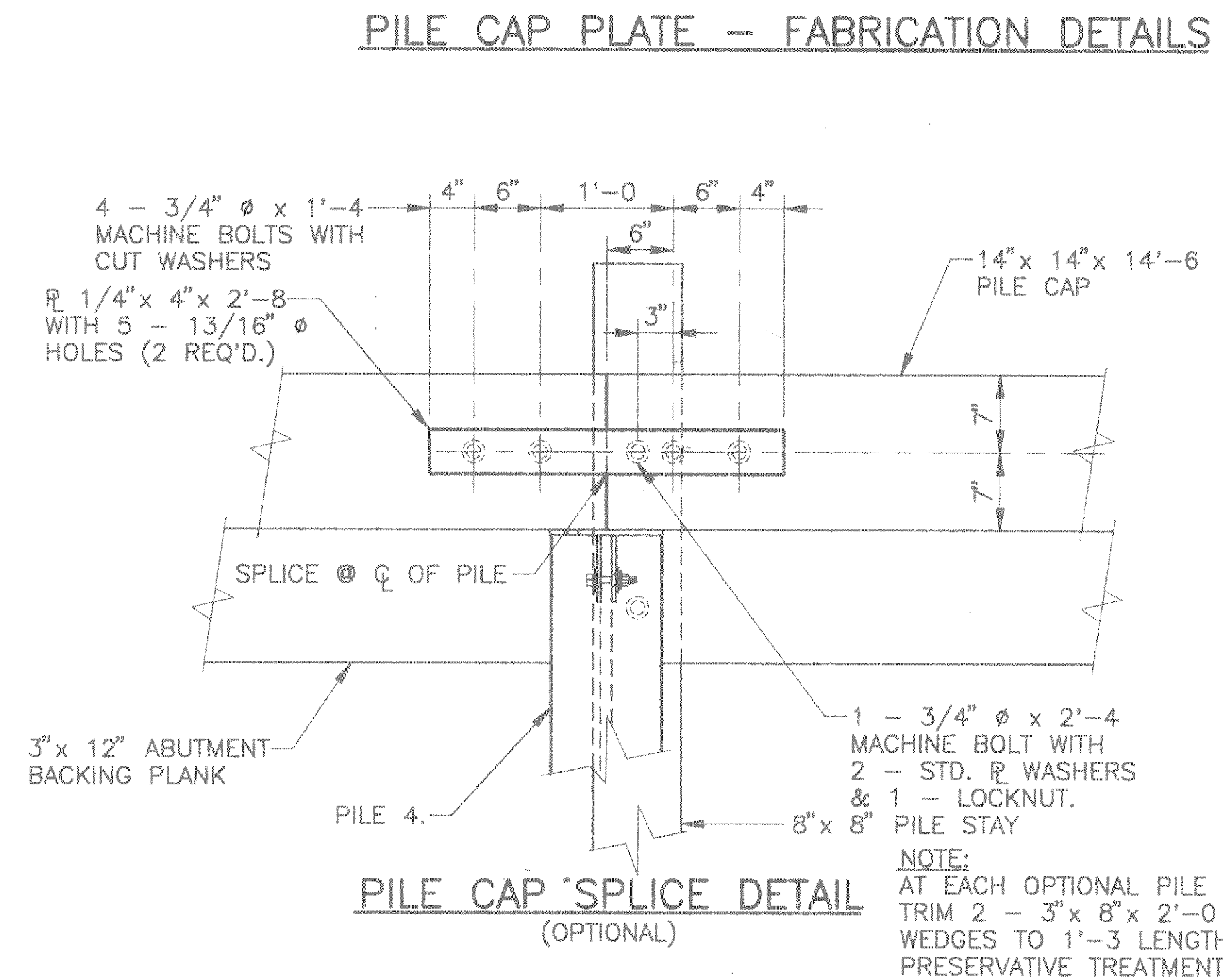
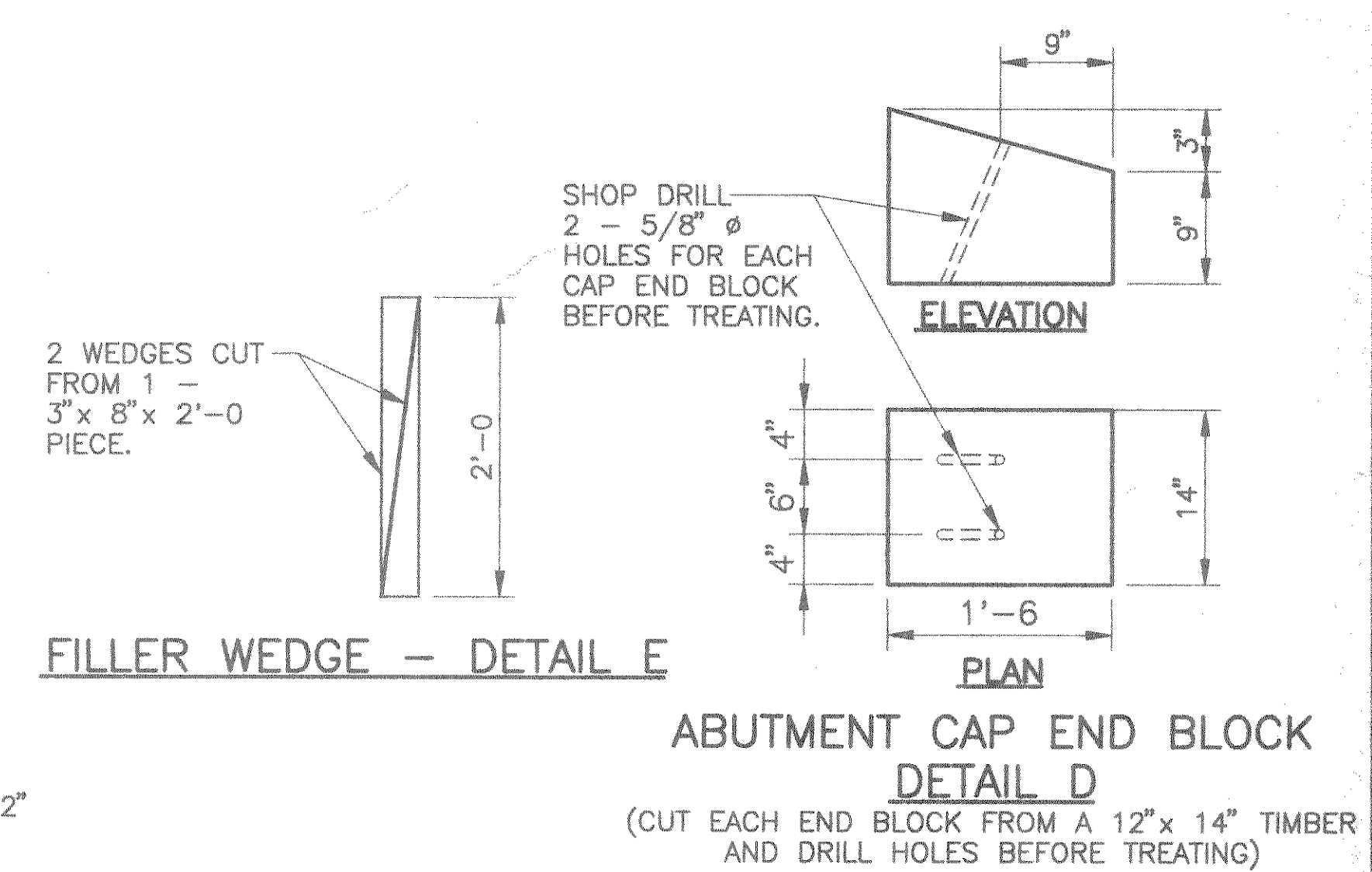
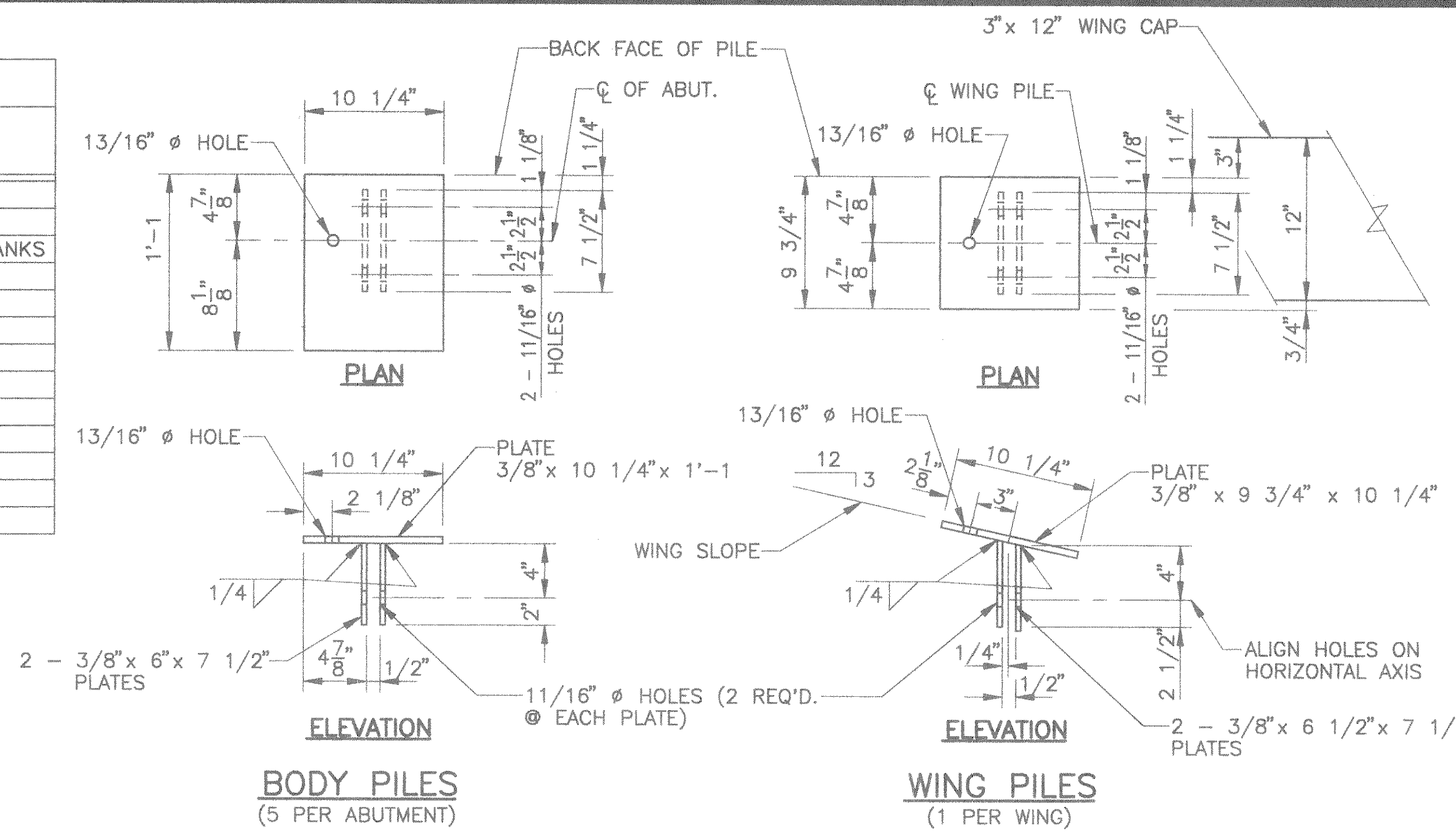
ALIGN PILE CAP PLATES ON TOP OF WING PILES TO FIT WING CAPS BEFORE CONNECTING PILE CAP PLATES TO PILES.

ALL PLATES TO BE STRUCTURAL STEEL PER CONSTRUCTION SPECIFICATIONS.

5/8" ϕ MACHINE BOLTS AND RELATED HARDWARE FOR CONNECTING STRUCTURAL STEEL ITEMS TO PILING ARE INCLUDED IN BID ITEM "STRUCTURAL CARBON STEEL".

BILL OF TREATED TIMBER (2 ABUTMENTS)				
ITEM	SIZE	NO. REQ'D.	MBM	
ABUTMENT BACKING	3" x 12" x 29'-0	12	1.044	
PILE STAYS	8" x 8" x 6'-0	6	.192	
ABUTMENT PILE CAP	14" x 14" x 29'-0	2	.947	
FILLER WEDGES	3" x 8" x 2'-0	6	.024	
TAPERED TIMBER FILLER	8" x 14" x 13'-0	2	.243	
ABUTMENT CAP BLOCK	12" x 14" x 1'-6	4	.084	
CLEAT @ ABUT. ENDS	4" x 10" x 8'-0	4	.107	
CLEAT FILLER	3" x 10" x 2'-0	4	.020	
WING PLANK (ALL WINGS)	3" x 12" x 7'-9	24	.558	
WING PLANK (TOP SLOPED)	3" x 12" x 8'-0	4	.096	
WING CLEAT (ALL WINGS)	3" x 12" x 6'-3	4	.075	
WING CLEAT (ALL WINGS)	3" x 12" x 7'-9	4	.093	
WING CAP (ALL WINGS)	3" x 12" x 9'-3	4	.111	
TOTAL TREATED TIMBER		M.B.M.	3.60	

* - TWO FILLER WEDGES CUT FROM ONE PIECE.
 ** - TWO TAPERED TIMBER FILLERS CUT FROM ONE PIECE.
 *** - TOP TWO WING PLANKS CUT FROM ONE PIECE.



No.	Date	Revision	By

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

STRUCTURE B-35-131

Const. Spec. Wl. '89 Drawn By RLR Piles Checked DHW

ABUTMENT DETAILS

SHEET 4 OF 6

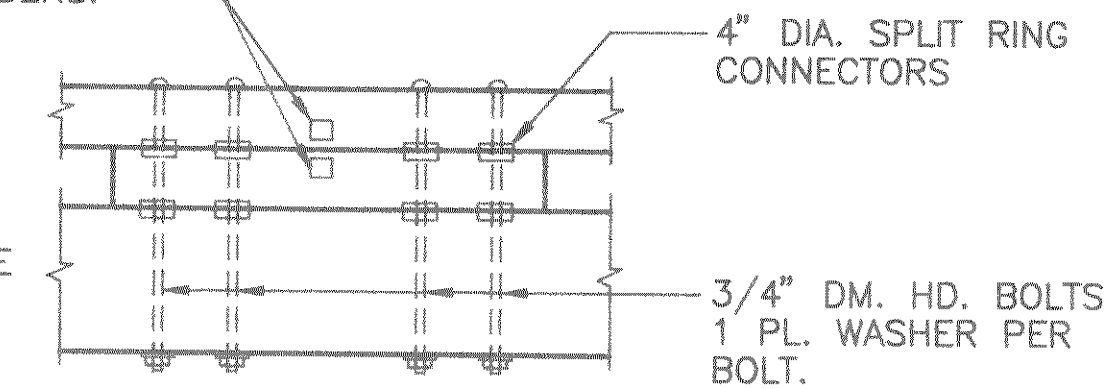
CONSTRUCTION NOTES

PANEL "A" IS THE FIRST PANEL TO BE PLACED IN ITS FINAL POSITION ON THE CAPS. DRILL 11/16" DIA. HOLES THRU DECK AND INTO THE CAPS AT THE LOCATIONS SHOWN AND DRIVE THE 3/4" DIA. DOME HEAD DRIVE SPIKES. NEXT PLACE PANEL "C" SO ITS UPPER SPLICE BLOCK IS OVER THE SPLICE BLOCK ON PANEL "A" AND DRAW TIGHT TOGETHER WITH A MINIMUM 3 TON LEVER HOIST. THEN DRILL HOLES IN LOWER SPLICE BLOCK OF PANEL "A" AND DRIVE THE 5/8" DIA. DOME HEAD DRIVE SPIKES. NEXT DRILL HOLE THRU THE DECK AND INTO THE CAP AND DRIVE THE 3/4" DOME HEAD DRIVE SPIKES. THEREAFTER, SUCCESSIVELY PLACE PANELS "C" AND "B" IN THE SAME MANNER USING THE LEVER HOIST TO DRAW EACH UNIT UP TIGHT.

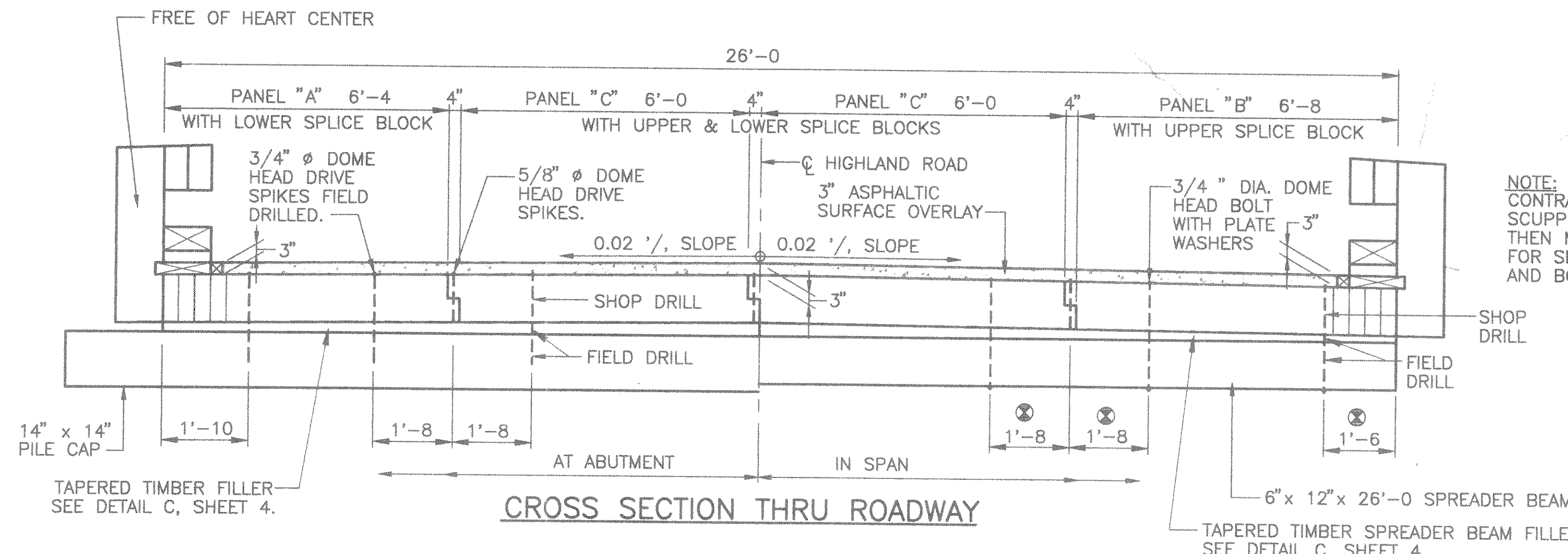
ALL HOLES WHERE DOME HEAD DRIVE SPIKES ARE USED ARE TO BE 1/16" SMALLER THAN SPIKE SIZE.

CURBS ARE TO BE PLACED SO TAGS SHOW FROM C/L ROADWAY. MATCH CURBS & SCUPPERS PER SHOP PLACED TAG NUMBERS.

NOTE: CONTRACTOR IS TO ASSEMBLE SCUPPER & CURB IN PLACE. THEN MARK & DRILL DECK FOR SPLIT RINGS, REASSEMBLE AND BOLT DOWN.



CURB & SCUPPER ASSEMBLY DETAIL



CROSS SECTION THRU ROADWAY

BILL OF TREATED TIMBER

ITEM	SIZE	NO. REQ'D.	MBM
PANEL "A"	12" x 6'-4" x 24'-0"	1	1.872
PANEL "B"	12" x 6'-8" x 24'-0"	1	1.968
PANEL "C"	12" x 6'-0" x 24'-0"	2	3.648
SPREADER BEAM	6" x 12" x 26'-0"	1	.156
SCUPPER BLOCK-INT. POSTS	6" x 12" x 3'-0"	2	.036
SCUPPER BLOCK-END POSTS	6" x 12" x 8'-3"	4	.198
RAIL POST	8" x 12" x 3'-11"	10	.313
SPACER BLOCK	6" x 8" x 10 1/2"	10	.035
DRIP PLATE	3" x 14" x 2'-3"	4	.032
SPREADER BEAM FILLER	SPLIT 8" x 6" x 13'-0"	1	.052
TIMBER EDGE PIECE	3" x 3" x 24'-0"	2	.036
CURB	6" x 12" x 24'-0"	2	.288
RAIL (GLU-LAM)	6" x 10 3/4" x 24'-0"	2	.258
TOTAL TREATED TIMBER		MBM	8.90

⊗ EYEBOLTS ARE IN PLACE AT SPREADER BEAM LOCATION WHEN SHIPPED AND ARE USED FOR LIFTING AND PLACING PANELS ON BRIDGE. EYEBOLTS ARE TO BE REMOVED AND REPLACED WITH 3/4" DIA. DOME HEAD BOLTS AND 1 - PLATE WASHER EACH TO FASTEN SPREADER BEAM. SPREADER BEAM IS FIELD DRILLED.

REQUIRED BOLT LENGTH

ITEM	LENGTH (IN.)
3/4" DIA. DM. HD. BOLTS - CURB TO FLOOR	26
5/8" DIA. DM. HD. DR. SPIKES - @ FLR. PANEL SPLICE	11 1/2
3/4" DIA. DM. HD. DR. SPIKES - PANEL TO CAP	26
3/4" DIA. DM. HD. BOLTS - SPREADER TO PANEL	32

BILL OF HARDWARE

ITEM	NO. REQ'D.
3/4" DIA. x 26" DM. HD. BOLT - RAIL TO POST	2 PER POST
1 1/4" x 26" DM. HD. BOLT W/ PL. WASHERS - POST	1 PER POST
3/4" DIA. x 30" DM. HD. DR. SPIKES - FLR. @ POST	3 PER POST
3/4" DIA. DM. HD. BOLTS - CURB TO FLR.	**
5/8" DIA. DM. HD. DR. SPIKES - @ FLR. PANEL SPLICE	39
3/4" DIA. DM. HD. DR. SPIKES - PANEL TO CAP	18
3/4" DIA. DM. HD. BOLTS - SPREADER TO PANEL	8
3/4" DIA. x 22" DM. HD. DR. SPIKES - CURB OVER ABUT.	8
40d. NAILS - TIMBER PAVING BLOCK & TIMBER DRIP PLATE	66
4" DIA. SPLIT RING CONNECTORS	8 PER POST
PLATE WASHER FOR 5/8" DIA. BOLTS	VARIES
PLATE WASHERS FOR 3/4" DIA. BOLTS	VARIES
3/8" DIA. R.S. SPIKES	22

** 4 PER INTERIOR POST AND 2 PER END POST

ABBREVIATIONS

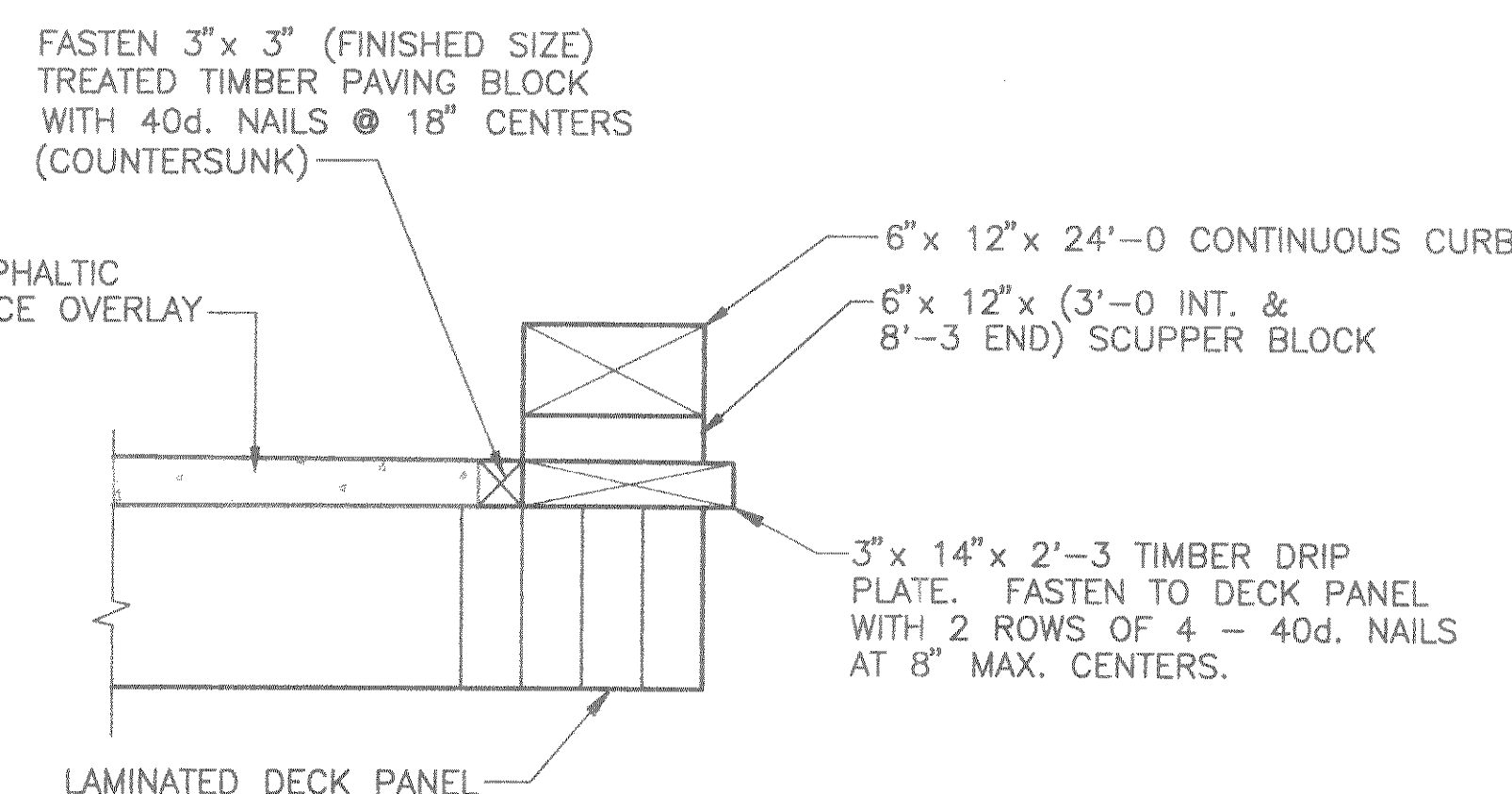
DM. - DOME PL. - PLATE
 HD. - HEAD FLR. - FLOOR
 DR. - DRIVE R.S. - RING SHANK

NOTES

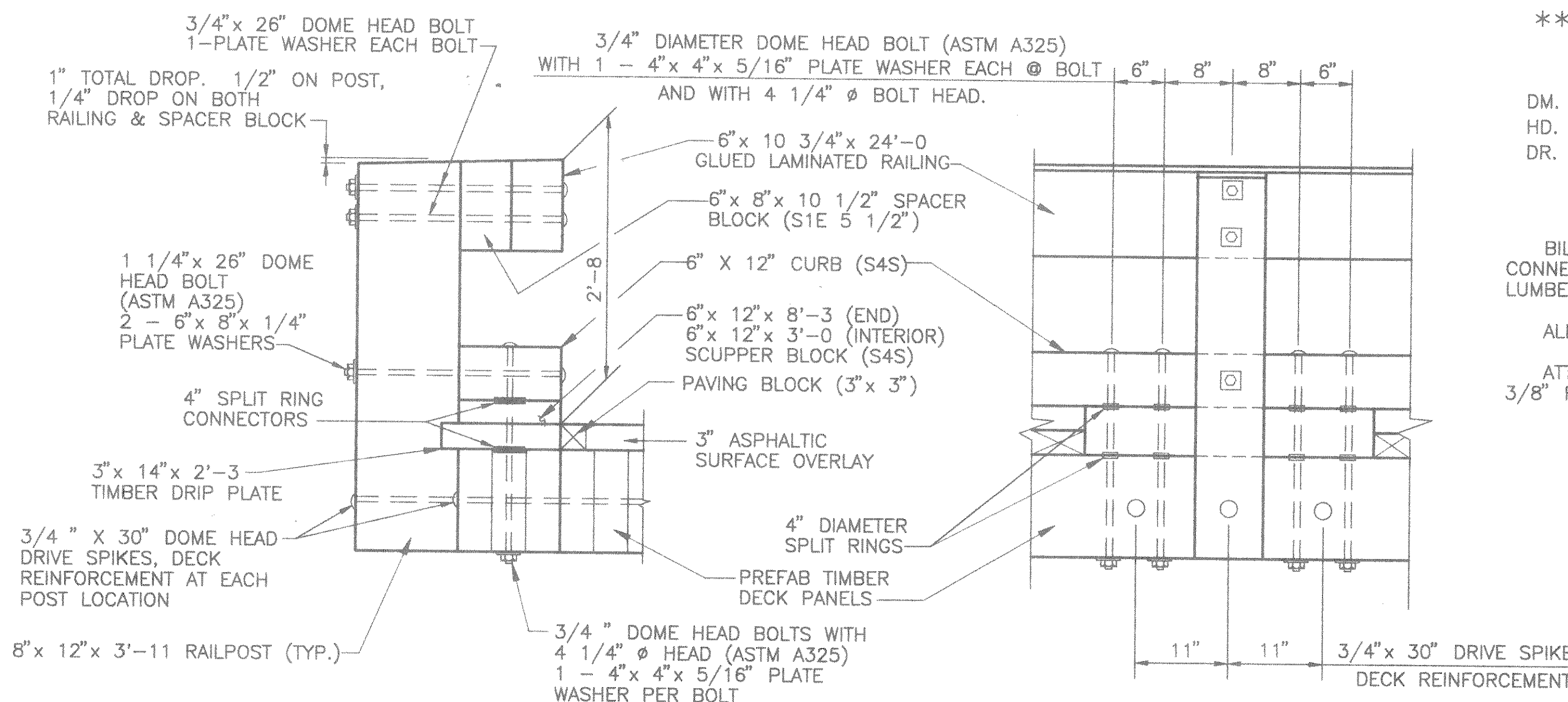
BILL OF HARDWARE IS FOR INFORMATION ONLY. ALL REQ'D. CONNECTORS AND HARDWARE ARE INCLUDED IN COST FOR "TREATED LUMBER AND TIMBER".

ALL TIMBER CONNECTORS AND HARDWARE SHALL BE GALVANIZED.

ATTACH SPREADER BEAM FILLER TO SPREADER BEAM WITH 3/8" R.S. SPIKES AT 2'-0" SPACES.



DECK SECTION AT CURB



SECTION AT RAILPOST

CURB AND SCUPPER DETAIL

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE			B-35-131
Const. Spec.	WI "89"	Drawn By	RLR Plans Checked <i>DHW</i>
SUPERSTRUCTURE			SHEET 5 OF 6

PREFABRICATED TIMBER PANEL NOTES

LAMINATE DECK PLANKS USING 3/8" ϕ x 8" & 3/8" ϕ x 15" & 3/8" ϕ x 11" GALVANIZED RING SHANK NAILS.

FOUR PLANKS SHALL BE FABRICATED FIRST, AS SHOWN, WITH 2-PLANK UNITS BEING ADDED TO MAKE 6'-0" WIDE SECTIONS. ADD (1) OR (2) 4" PLANKS RESPECTIVELY TO MAKE 6'-4 AND 6'-8 PANELS.

SPLICE BLOCK SHALL BE FASTENED WITH 11" GALVANIZED RING SHANK NAILS.

DRILL 5/8" ϕ HOLES FOR DOME HEAD DRIVE SPIKES IN UPPER SPLICE BLOCK AS SHOWN.

DRILL 13/16" HOLES FOR 3/4" EYEBOLTS FOR HANDLING AND FOR FASTENING SPREADER BEAM WHEN ERECTED.

PLANK TO BE PREBORED BEFORE BEING TREATED.

RING SHANK NAILS SHALL BE SET IN PREBORED HOLES OF 2 & 4 PLANK UNITS BEFORE DRIVING.

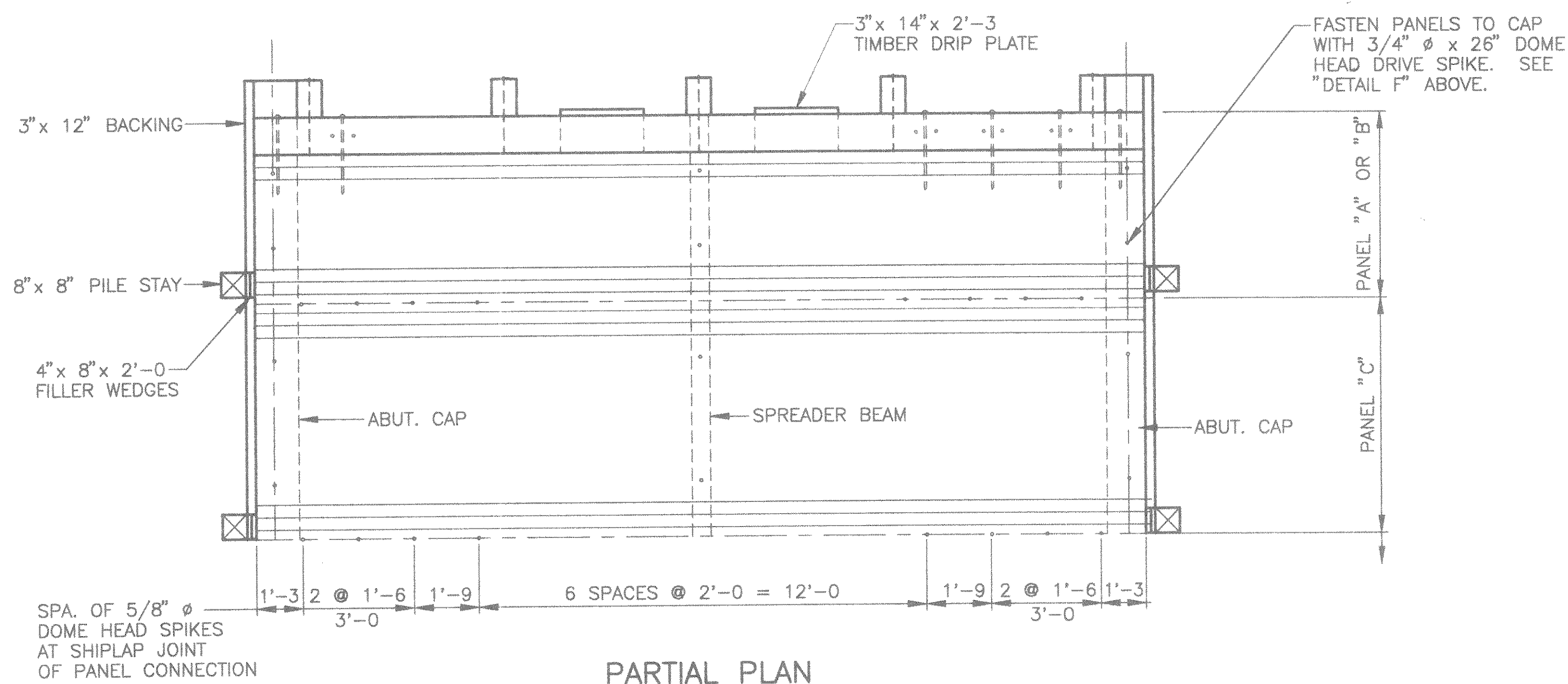
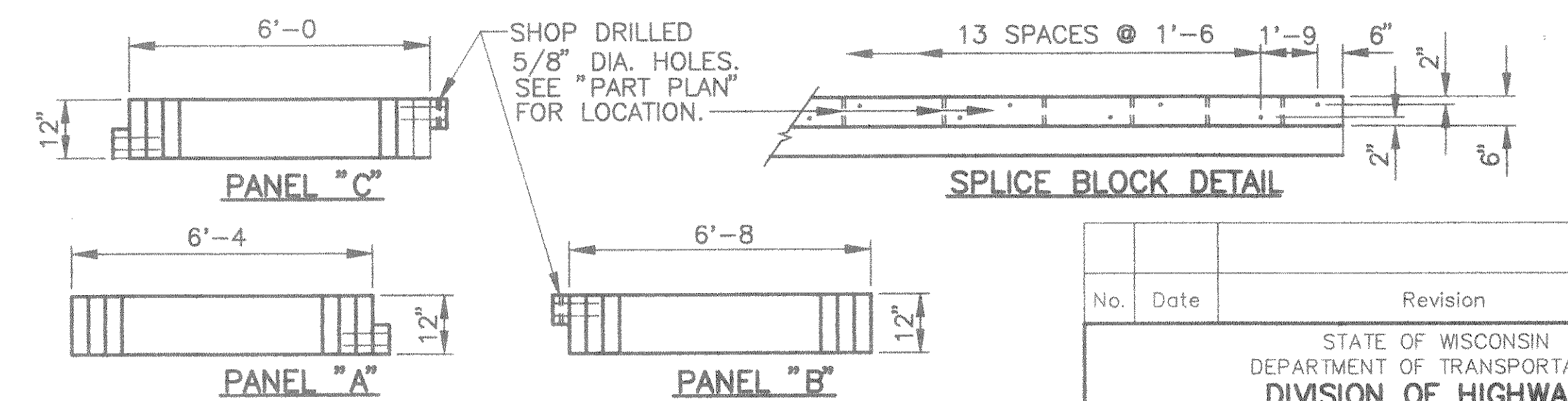
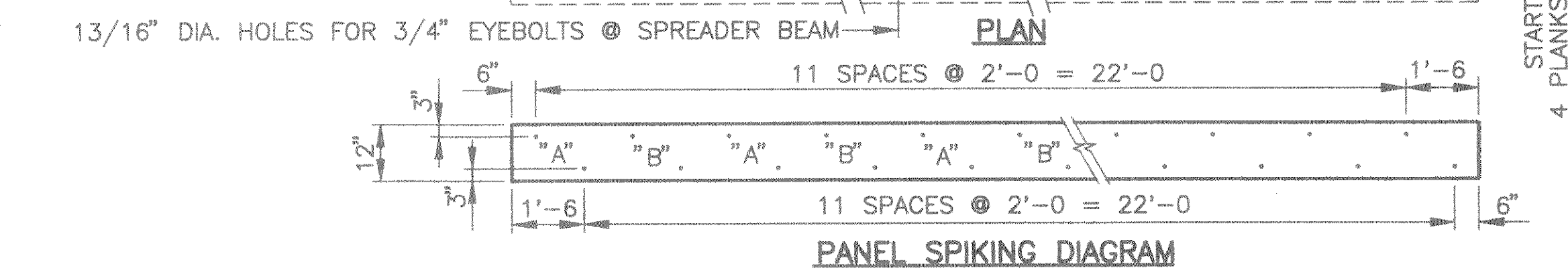
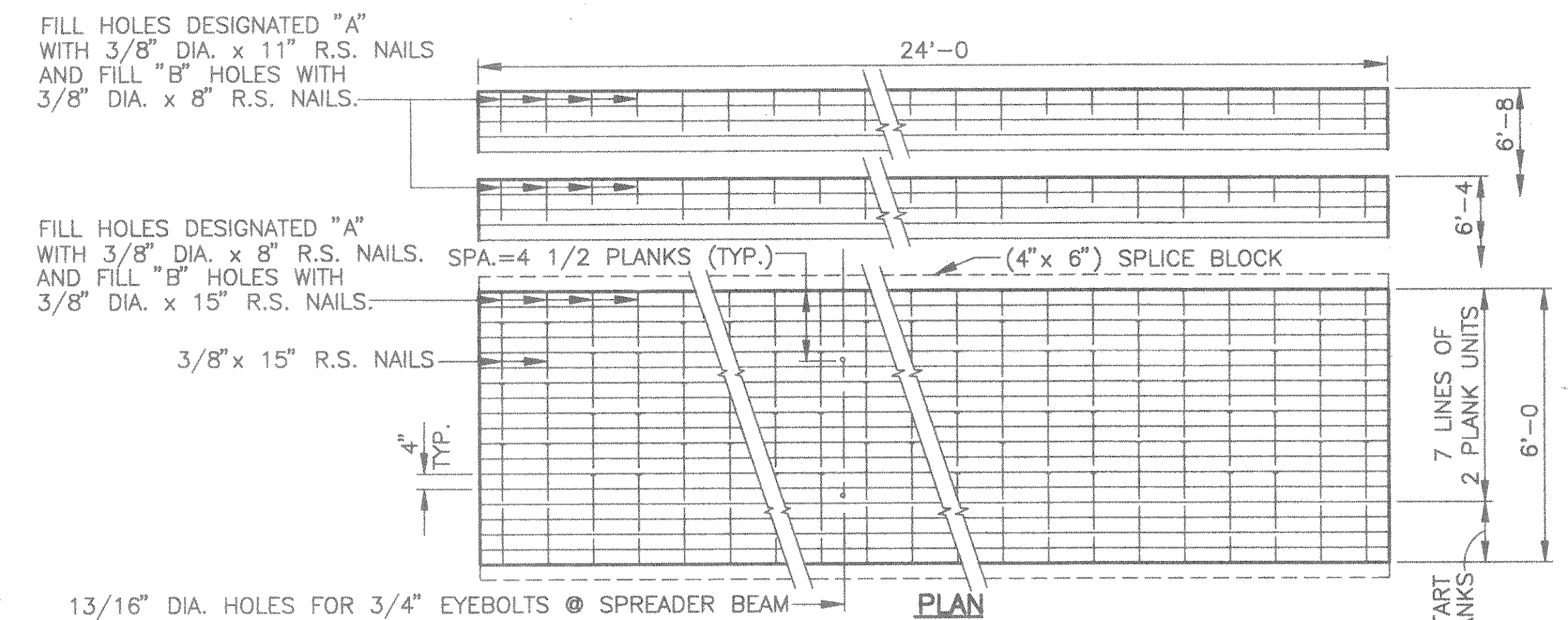
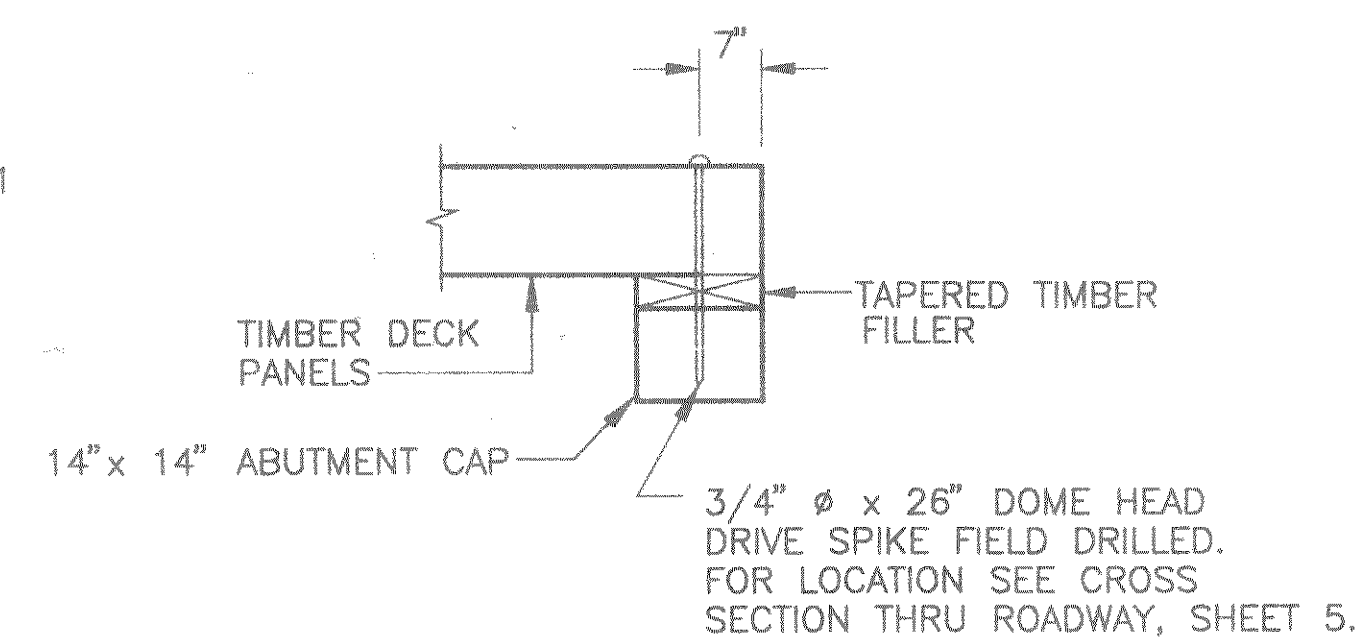
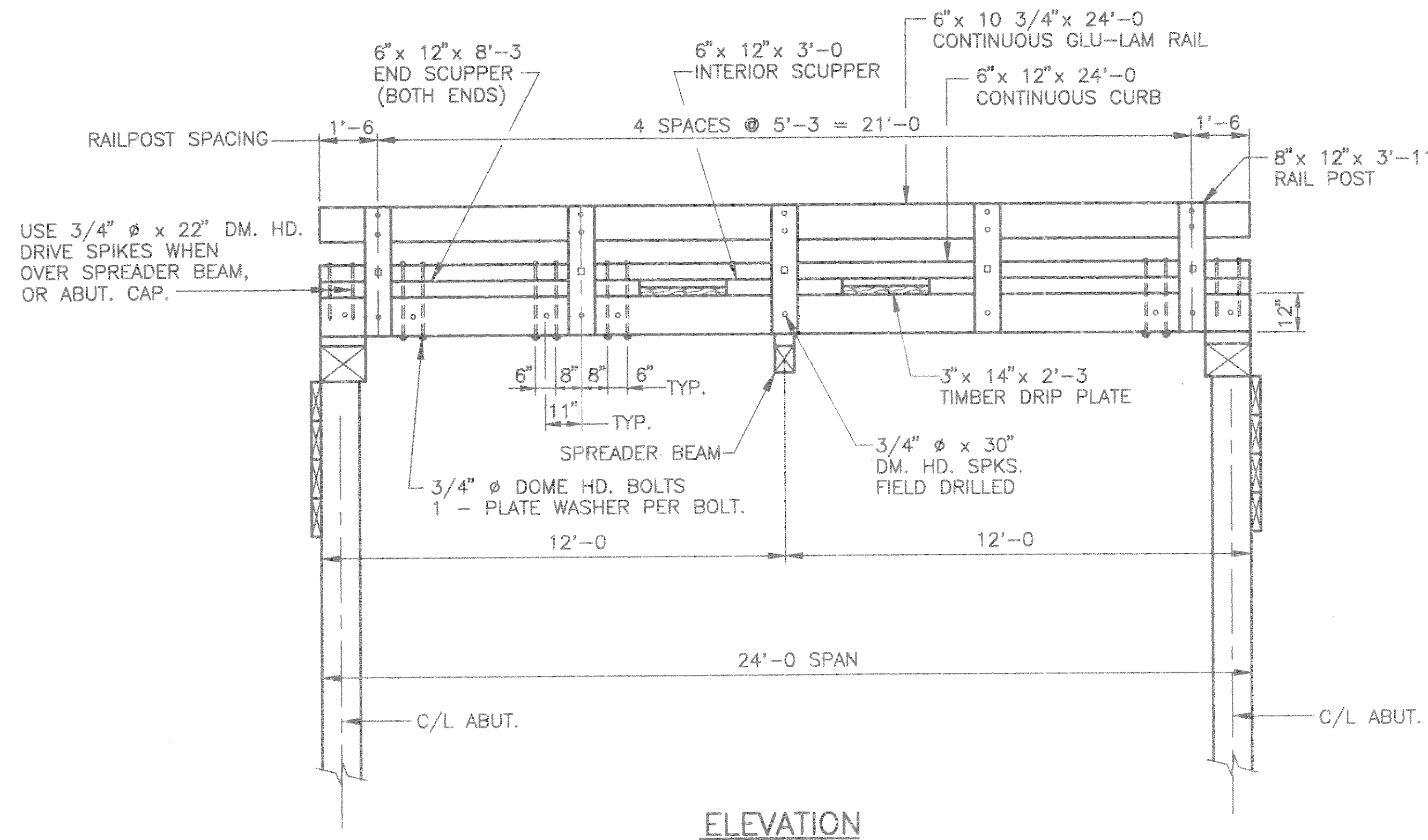
NAIL DRIVING SHALL BE DONE WITH AN AIR HAMMER SO THAT PLANKS ARE DRIVEN TO A TIGHT FIT TO MAKE DIMENSIONS OF PANELS.

ALL PANELS SHALL BE SURFACED ONE SIDE (S1S) SO THAT PANELS WILL BE 6'-0, 6'-4, OR 6'-8 AFTER FABRICATION.

ALL PLANKS SHALL BE CREOSOTE TREATED AS PER SEC. 507 OF THE STANDARD SPECIFICATIONS.

ALL HARDWARE SHALL BE GALVANIZED, INCLUDING RING SHANK NAILS.

AS AN ALTERNATE, THE DECK MAY BE FABRICATED IN PLACE AND THE SPLICE BLOCK PLANKS & DOME HEAD DRIVE SPIKES OMITTED. ASSEMBLY TO BEGIN AT C/L OF BRIDGE, PROCEEDING TO FINISH AT EACH FACIA. ALL OTHER NOTES APPLY.



No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE		B-35-131	
Const. Spec.	WI "89"	Drawn By	RLR
		Plans Checked	DHW
SUPERSTRUCTURE DETAILS			SHEET 6 OF 6