

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
9411-01-71	BRS 1327(2)	1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT STH 64 — STH 107

(COPPER RIVER BRIDGE & APPROACHES)

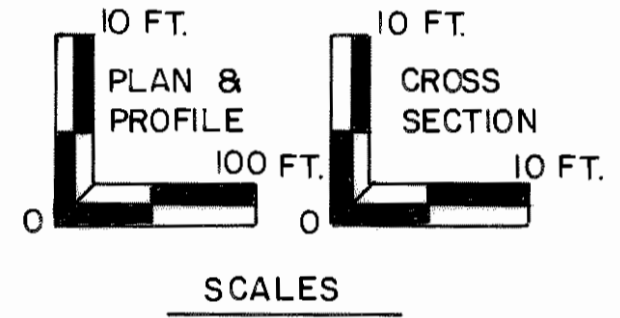
C.T.H. "E" LINCOLN COUNTY

STATE PROJECT NUMBER
9411-01-71

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TOTAL SHEETS = 27



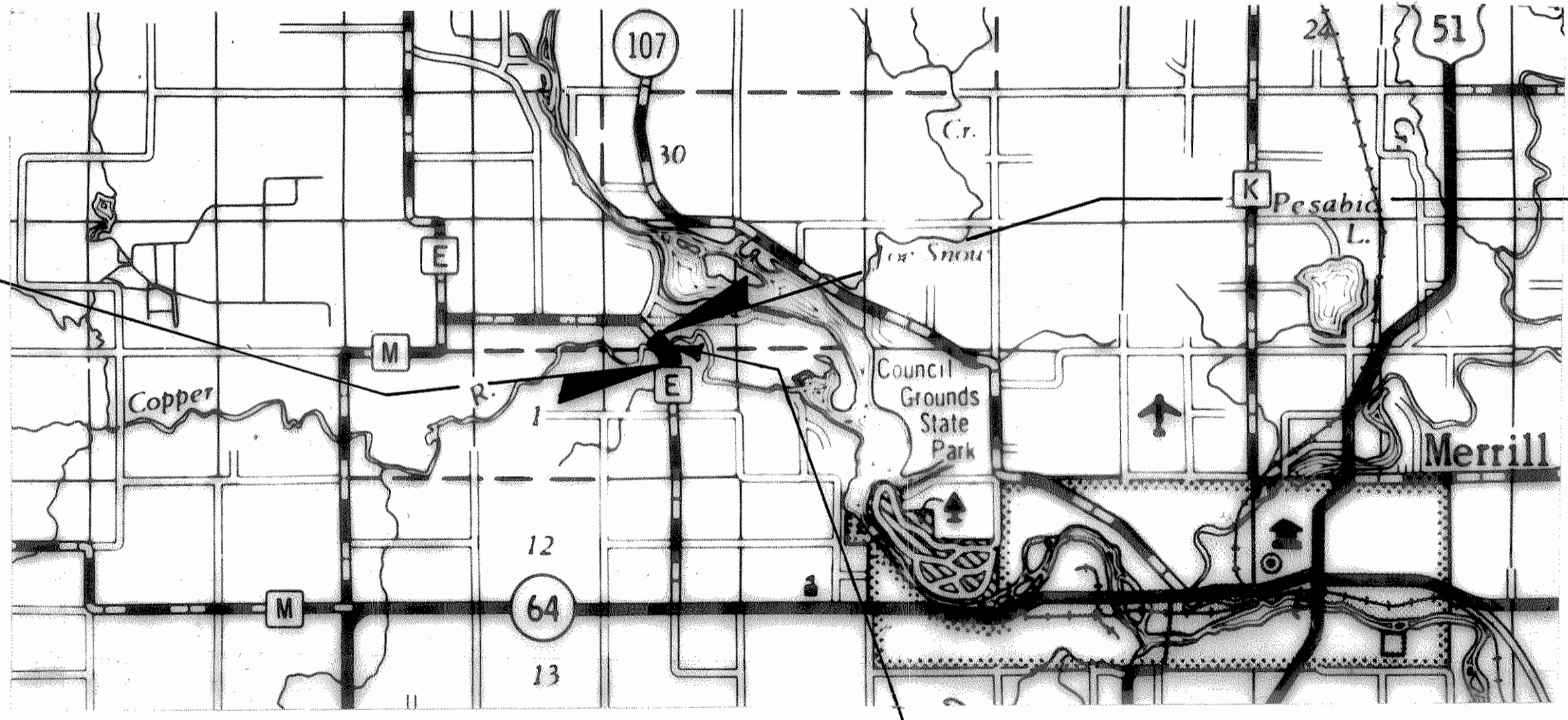
Design Designation

A.D.T. (1982)	=	480
A.D.T. (2002)	=	880
D.H.V. (2002)	=	106
D.	=	55 - 45 %
T. (% ADT)	=	6 %
V.	=	50 MPH

**BEGIN PROJECT
STATION 10+00**

N = 500,100 (±100')
E = 2,055,150 (±100')
697.90' SOUTH AND
1.41' WEST OF THE
NORTH ¼ CORNER,
SECTION 6, T-31-N, R-6-E.

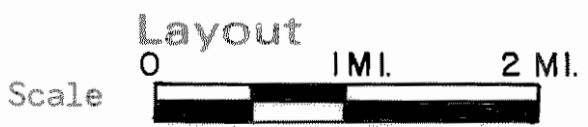
T-32-N
T-31-N



**END PROJECT
STATION 23+00**

Conventional Signs

County Line	-----	Caution Symbol (Combustible fluids under pressure)	
Township or Range Line	-----	Railroads	+++++
Section Line	-----	Fence	---X---X---
Corporate or City Limits	-----	Culverts in Place	-----
Property line	-----	Culverts Required	-----
Lot Line	-----	Power Pole	-----
Existing Right of Way Line	-----	Telephone or Telegraph Pole	-----
New Right of Way Line	-----	Right of Way Markers	-----
Base or Survey Line	-----	Marsh	-----
Slope Intercept	-----	Wooded Area	-----
Existing Roadway or Private Entrance	-----	Grade Elevation	-----
		Telephone Riser	-----



Total Net Length of Centerline = 0.245 Mi. (RURAL)

ALL COORDINATES SHOWN ON THIS PLAN ARE REFERENCED TO WISCONSIN COORDINATE SYSTEM CENTRAL ZONE AND SCALED FROM THE ALEXANDER LAKE QUADRANGLE FOR IDENTIFICATION PURPOSES ONLY.

APPROVED FOR
LINCOLN COUNTY
BY
Michael L. Hemp
DATE 8-15-83 COUNTY HIGHWAY COMMISSIONER

PLANS PREPARED
BY
MEAD & HUNT, INC.
CONSULTING ENGINEERS
MADISON, WISCONSIN
Gary A. Goetz Feb 9, 1982
Rajani L. Suresh Feb 9, 1982

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
Surveyor: *M.E.H.* District Checker: *D.D.K.*
Designer: *M.E.H.* C.O. Checker: *Blc*
District Supervisor: *R.J.S.* C.O. Coordinator: *L.A.S.*

Approved: *R.D. Schindler*
Date 11/11/85 District Transportation Director

Approved: *D.J. Ahlund*
Date 11-29-85 Chief Design Engineer

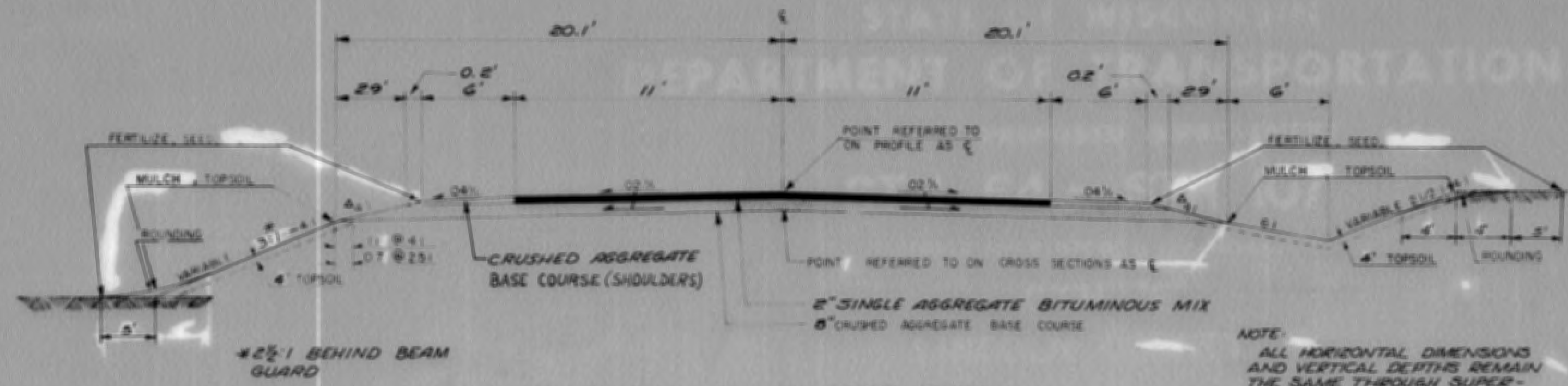
Approved: *E.J. Byrkit*
Date 12/2/85 Director of Development

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 5 WISCONSIN DIVISION

Approved: _____
Date _____ Division Administrator

LINCOLN
9411-1-71
Feb 18
NET.

STATE PROJECT NUMBER	SHEET NO.
941-01-71	2.0
TYPICAL CROSS SECTION FOR C.T.H. "E" LINCOLN COUNTY	



TYPICAL ROADWAY CROSS SECTION
STA 10+00 TO STA 23+00

NOTE:
ALL HORIZONTAL DIMENSIONS AND VERTICAL DEPTHS REMAIN THE SAME THROUGH SUPER-ELEVATED SECTIONS.
INSLOPE VARIES FROM 4.77:1 ON THE HIGH SIDE TO 3.05:1 ON THE LOW SIDE.

GENERAL NOTES

FILL, AS SHOWN ON THE PLAN SHEETS, PERTAINS TO EMBANKMENT CONSTRUCTED FROM BORROW EXCAVATION AND/OR UNCLASSIFIED EXCAVATION. THE SHRINKAGE ALLOWANCE USED TO COMPUTE THE VOLUME OF MATERIAL NECESSARY TO COMPLETE THE FILL WAS 25% FOR UNCLASSIFIED EXCAVATION SOUTH OF STRUCTURE AND 20% FOR UNCLASSIFIED EXCAVATION NORTH OF STRUCTURE. EROSION BALES ARE TO BE DISTRIBUTED AS DETERMINED BY THE ENGINEER.

SEED MIXTURE NUMBER 1 SHALL BE USED.
DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCLUSIVE OF THE ROADBED, ARE TO BE FERTILIZED AND SEEDED AS DIRECTED BY THE ENGINEER.

BEARINGS SHOWN ON THE PLANS ARE MAGNETIC BEARINGS TO THE NEAREST MINUTE.

CURVE DATA IS BASED ON THE ARC DEFINITION.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

SAWING EXISTING PAVEMENT ON THIS PROJECT IS CONSIDERED INCIDENTAL TO OTHER CONTRACT ITEMS.

STANDARD ABBREVIATIONS

- | | | |
|---------------------|------------------------|-----------------------------|
| LT = LEFT | L.F. = LINEAR FEET | PI. = POINT OF INTERSECTION |
| RT = RIGHT | IN = INCHES | PC = POINT OF CURVE |
| CP = CULVERT PIPE | SQ. YD. = SQUARE YARDS | PT. = POINT OF TANGENT |
| PP = POWER POLE | ELEV. = ELEVATION | BIT = BITUMINOUS |
| TP = TELEPHONE POLE | R/W = RIGHT OF WAY | C = CENTERLINE |
| Δ = DELTA ANGLE | REQ'D = REQUIRED | T = TRANSIT LINE |
| D = DEGREE | STA. = STATION | PL. = PROPERTY LINE |
| R = RADIUS | V.C. = VERTICAL CURVE | |
| T = TANGENT | PE = PRIVATE ENTRANCE | |
| L = LENGTH | FE = FIELD ENTRANCE | |

STANDARD DETAIL DRAWINGS

- 8E8-1 TYPICAL INSTALLATIONS OF EROSION BALES
- 8F1-9 APRON ENDWALLS FOR CULVERT PIPE AND PIPE ARCH
- 12A3-4 NAME PLATE STRUCTURES
- 14B2-3a/3b CLASS "A" STEEL PLATE BEAM GUARD (TWO SHEETS)
- 15C1-7 CONSTRUCTION BARRICADES AND STANDARD SIGNS

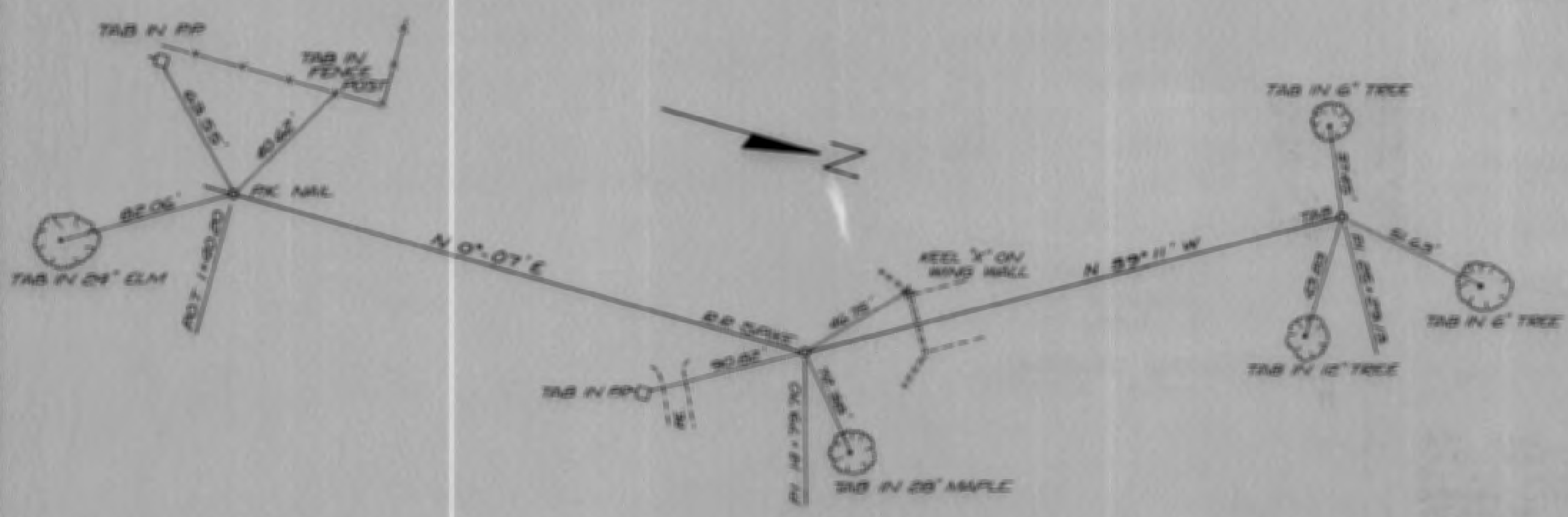
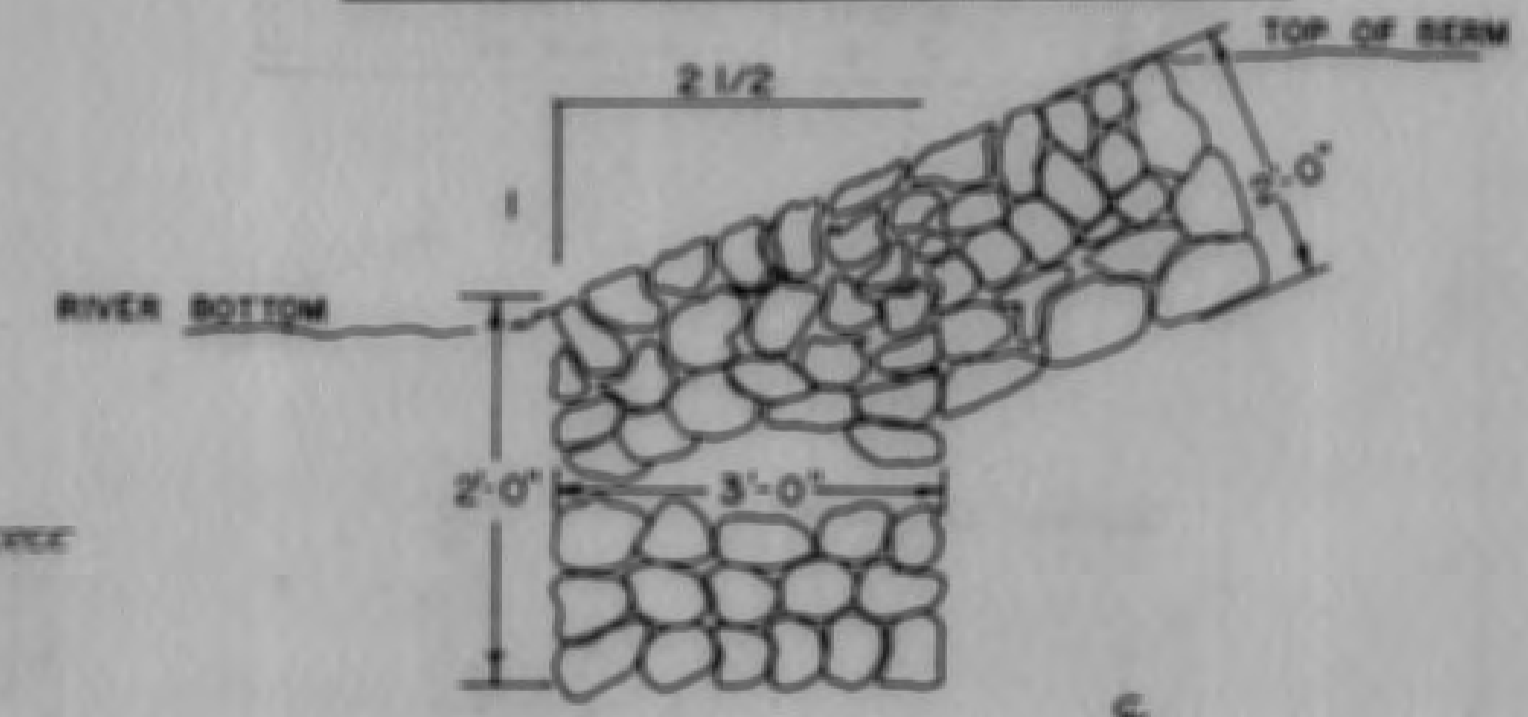


PRIVATE ENTRANCE DETAIL

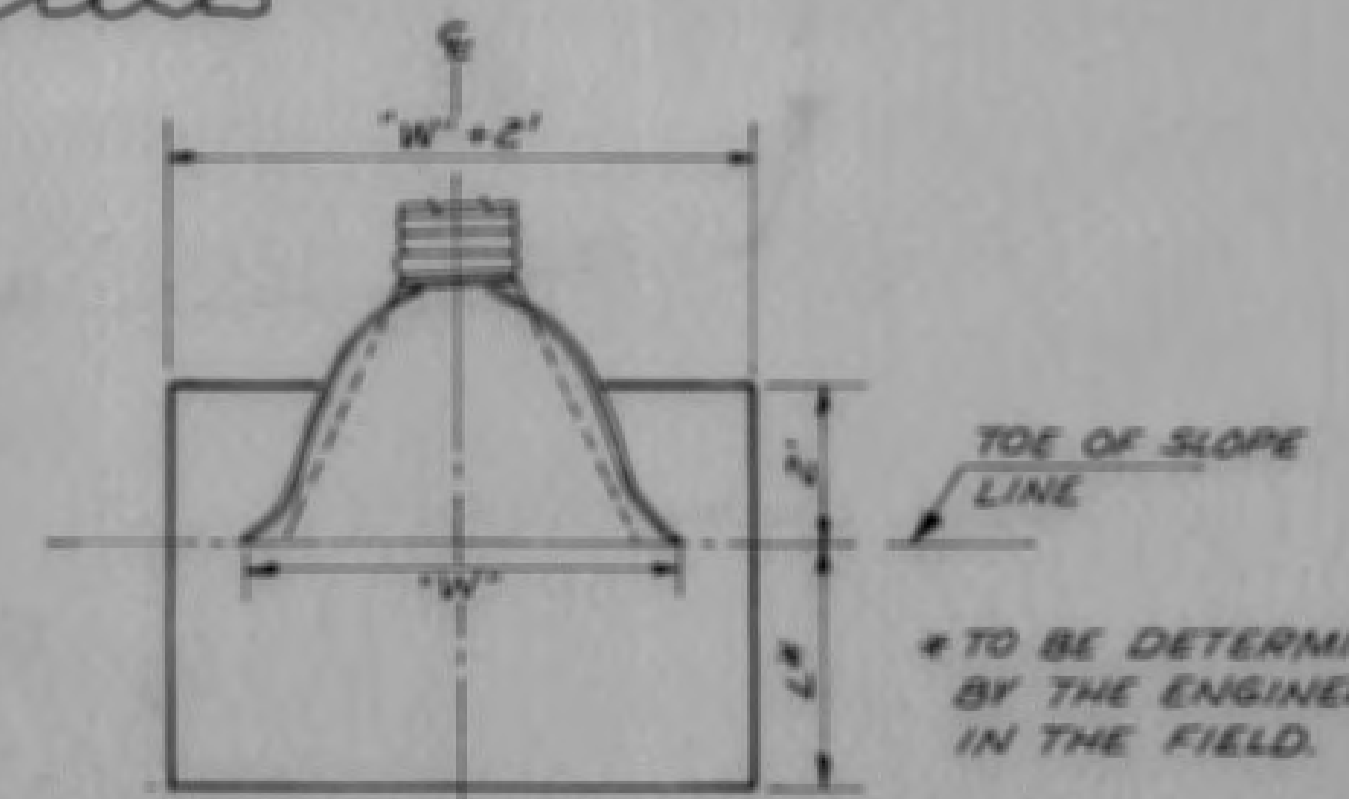


PROFILE OF ENTRANCES

TYPICAL SECTION OF RIPRAP DETAIL



ALIGNMENT DIAGRAM & CONSTRUCTION TIES



SOD AT PIPE END

UTILITIES

GENERAL TELEPHONE COMPANY
413 MC CLELLAN ST.
WAUSAU, WISCONSIN 54401
C. R. WAGNER
1-715-842-2011
WISCONSIN PUBLIC SERVICE CORP
300 E 2nd ST.
MERRILL, WI. 54452
GENE BONDIOLI
1-715-536-5591

* TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

SCHEDULE OF LANDS AND INTERESTS REQUIRED

PARCEL NUMBER	OWNER	INTEREST REQUIRED	L.H.E. ACRES	ACRES REQUIRED			TOTAL REMAINING ACRES	OPERATIONS PROJECT NO.
				NEW R/W REQUIRED	EXISTING R/W	TOTAL R/W REQUIRED		
1	EMIL & ESTHER OESTREICH	FEE	0.02	0.30	0.30	0.60		9411-01-00
2	LLOYD & VIRGINIA OESTREICH	"	—	0.01	0.10	0.11		"
3	RALPH E. SCHULTZ	"	—	0.01	—	0.01		"
4	REINHARDT LANDS, INC.	"	0.28	0.28	0.24	0.52		"
5	DONALD L. & CAROL A. KAATZ	"	—	0.08	0.14	0.22		"
6	JOSEPH & CAROL ALERY	"	0.15	0.34	0.84	1.18		"
7	COUNTRY SQUIRE LANDS, INC.	"	0.64	—	—	—		"
8	GENERAL TELEPHONE CO.	RELEASE OF RIGHTS	—	—	—	—		"

ALL R/W IS REFERENCED TO THE CONSTRUCTION & BEARINGS SHOWN ON THE PLAT ARE MAGNETIC BEARINGS TO THE NEAREST MINUTE. ALL COORDINATES SHOWN ON THIS PLAT ARE REFERENCED TO THE WISCONSIN COORDINATE SYSTEM, CENTRAL ZONE, AND SCALED FROM THE ALEXANDER LAKE QUADRANGLE FOR IDENTIFICATION PURPOSES ONLY.

REVISION DATE	R/W PROJECT NUMBER 9411-01-00	SHEET NUMBER
	FEDERAL PROJECT NUMBER	
PLAT OF RIGHT OF WAY REQUIRED FOR C.T.H. "E" LINCOLN COUNTY		
SCALE 0 100 200 Ft.	DATE 8-11-83	
CONSTRUCTION PROJECT NUMBER 9411-01-71		4.0
APPROVED FOR LINCOLN COUNTY BY <i>[Signature]</i> DATE COUNTY HIGHWAY COMMISSIONER		

LEGEND

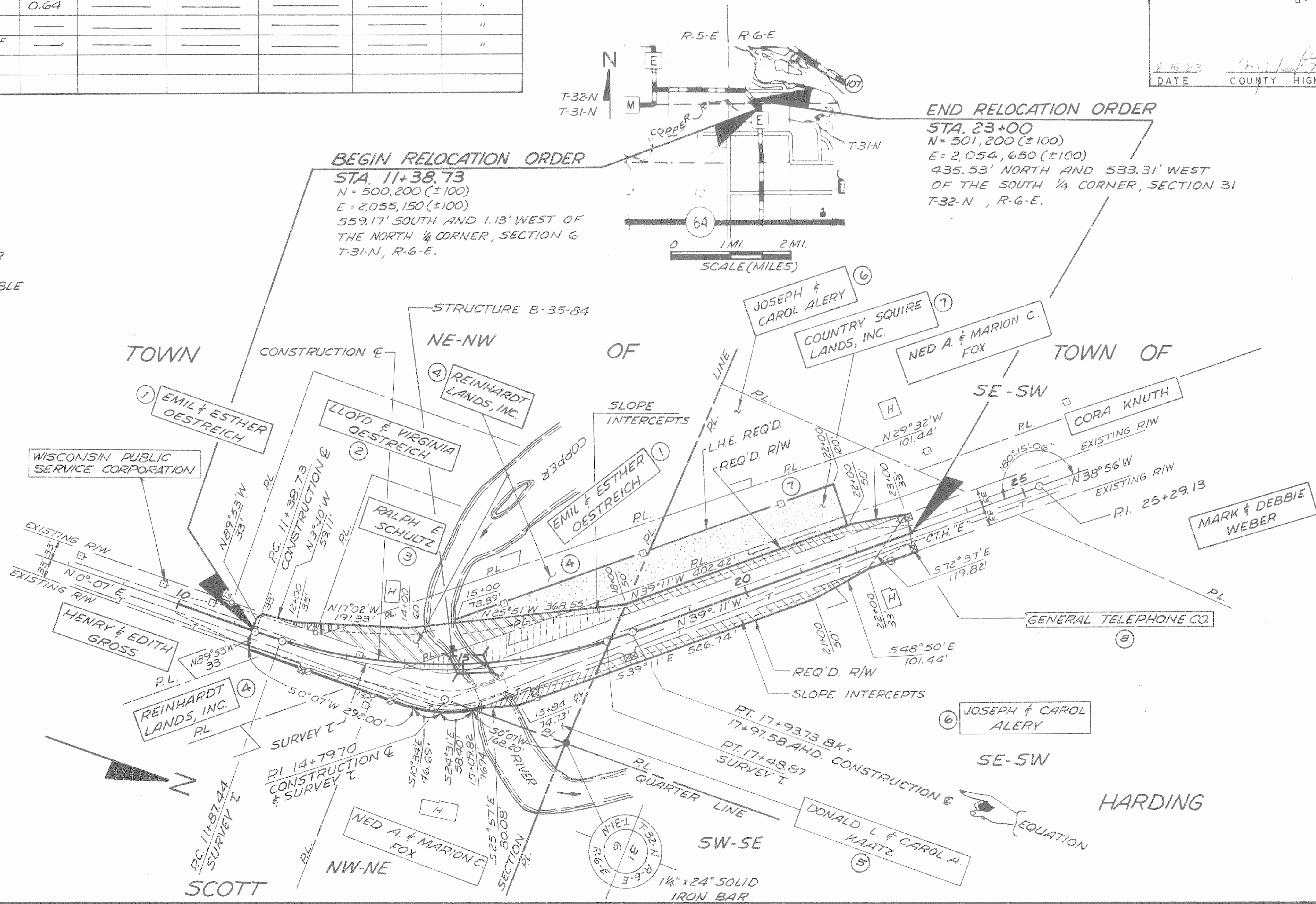
- ☐ TELEPHONE POLE
- ⊥ UNDERGROUND TELEPHONE RISER
- ☐ POWER POLE
- T — UNDERGROUND TELEPHONE CABLE

STANDARD ABBREVIATIONS

- PL. PROPERTY LINE
- ⊥ TRANSIT LINE
- ⊕ CENTERLINE
- R/W RIGHT OF WAY
- L.H.E. LIMITED HIGHWAY EASEMENT
- REQ'D REQUIRED

CURVE DATA

CONSTRUCTION &	SURVEY I
PI. = 14+79.70	PI. = 14+79.70
Δ = 39°-18'-00"	Δ = 39°-18'-00"
D = 6°-00'	D = 7°-00'
R = 954.93'	R = 818.51'
T = 340.97'	T = 292.26'
L = 655.00'	L = 561.43'
E = 59.05'	E = 50.61'

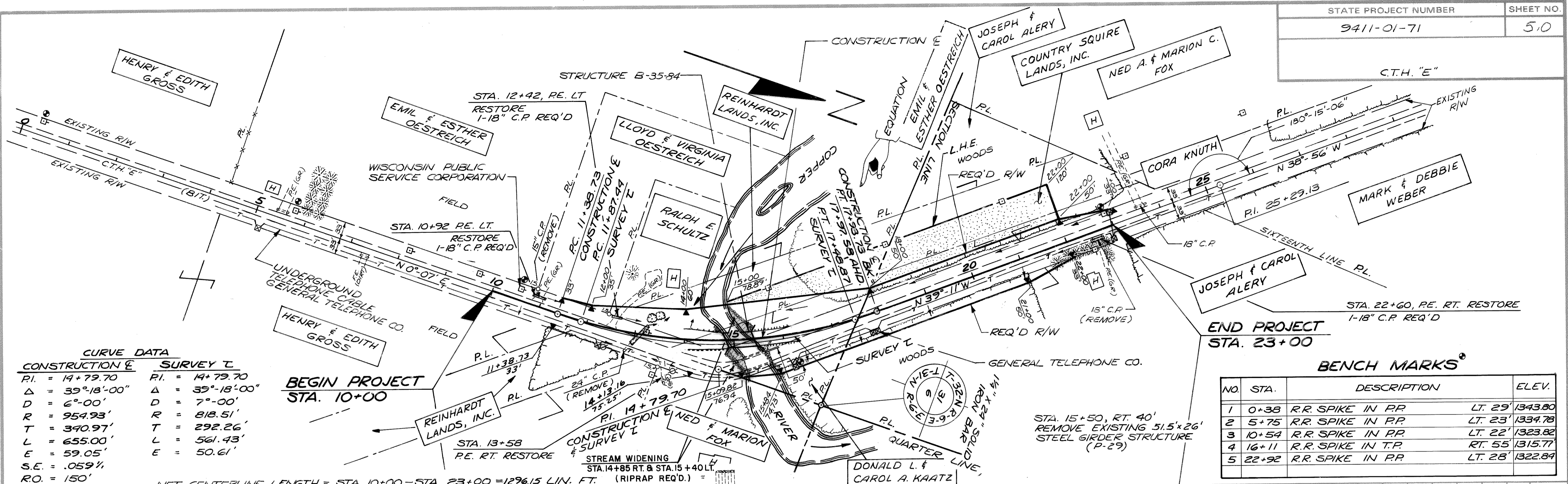


BEGIN RELOCATION ORDER

STA. 11+38.73
N = 500,200 (±100)
E = 2,055,150 (±100)
559.17' SOUTH AND 1.13' WEST OF THE NORTH 1/4 CORNER, SECTION 6 T-31-N, R-6-E.

END RELOCATION ORDER

STA. 23+00
N = 501,200 (±100)
E = 2,054,650 (±100)
435.53' NORTH AND 533.31' WEST OF THE SOUTH 1/4 CORNER, SECTION 31 T-32-N, R-6-E.

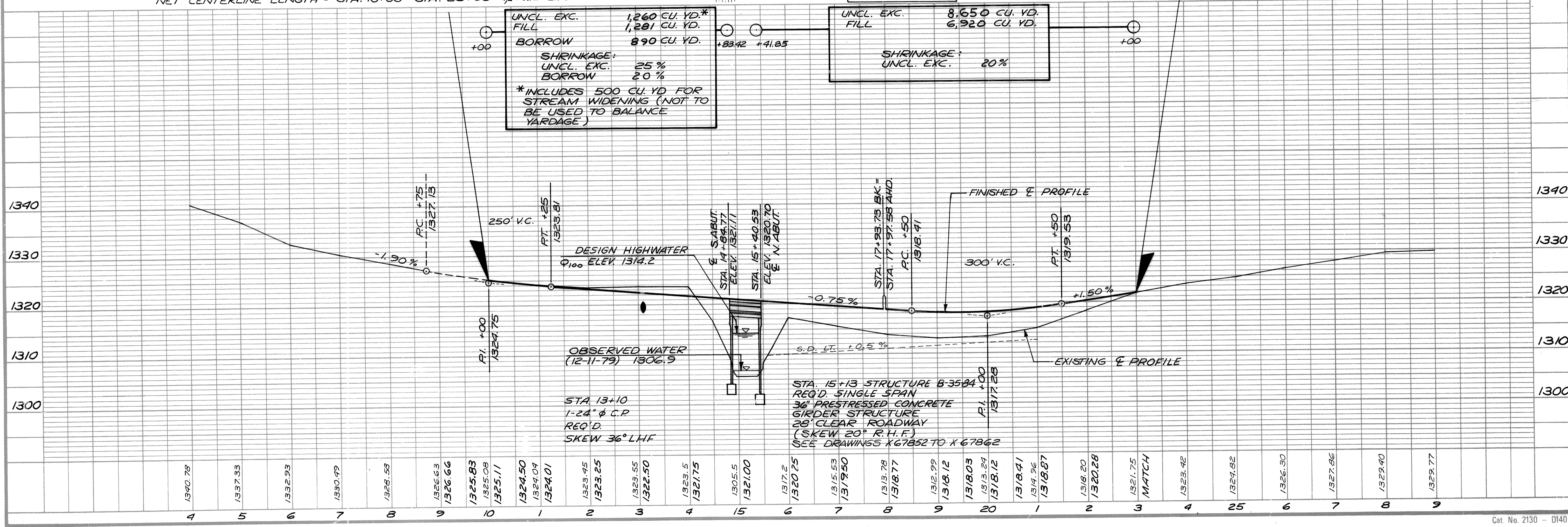


CURVE DATA

CONSTRUCTION E	SURVEY I
PI. = 14+79.70	PI. = 14+79.70
Δ = 39°-18'-00"	Δ = 39°-18'-00"
D = 6°-00'	D = 7°-00'
R = 954.93'	R = 818.51'
T = 340.97'	T = 292.26'
L = 655.00'	L = 561.43'
E = 59.05'	E = 50.61'
S.E. = .059%	
R.O. = 150'	

BENCH MARKS

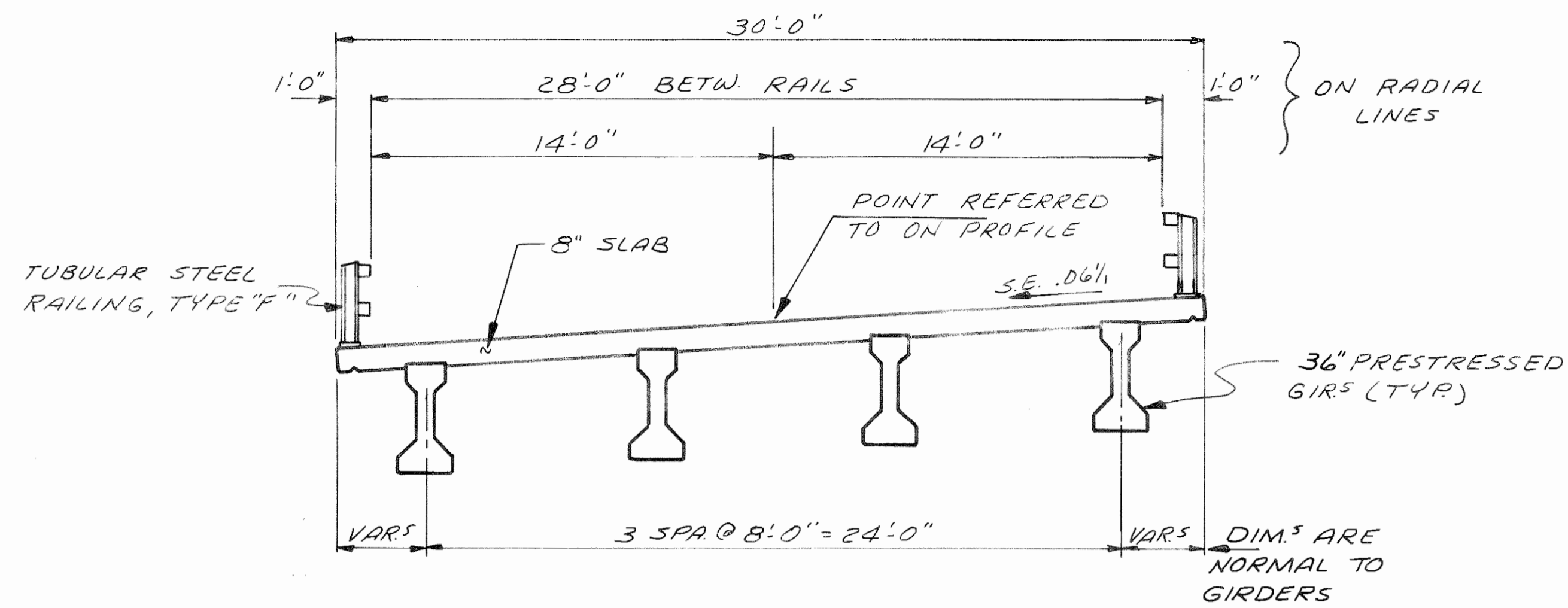
NO.	STA.	DESCRIPTION	ELEV.
1	0+38	R.R. SPIKE IN PP	LT. 29' 1343.80
2	5+75	R.R. SPIKE IN PP	LT. 23' 1334.78
3	10+54	R.R. SPIKE IN PP	LT. 22' 1323.82
4	16+11	R.R. SPIKE IN T.P.	RT. 55' 1315.71
5	22+92	R.R. SPIKE IN PP	LT. 28' 1322.84



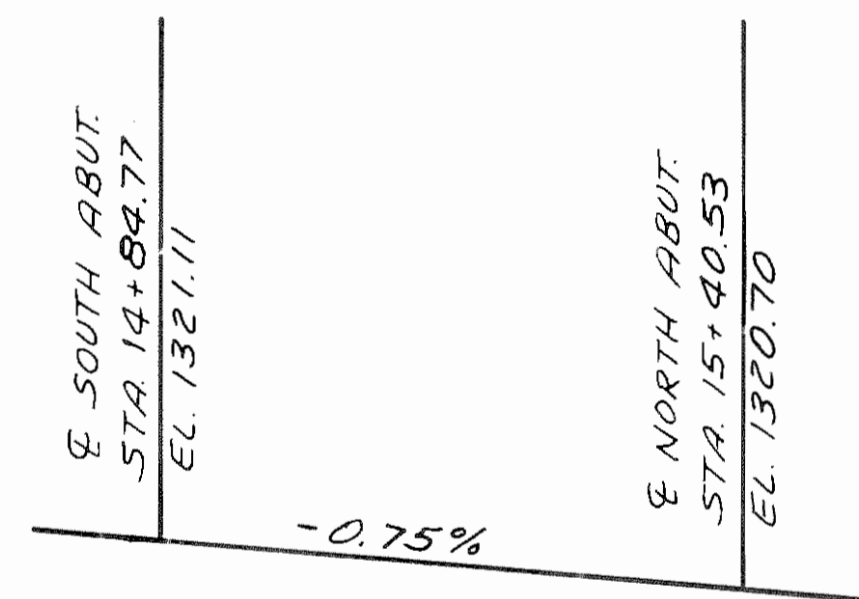
UNCL. EXC. FILL	1,260 CU. YD.*
BORROW	890 CU. YD.
SHRINKAGE:	
UNCL. EXC. BORROW	25%
	20%

*INCLUDES 500 CU. YD. FOR STREAM WIDENING (NOT TO BE USED TO BALANCE YARDAGE)

UNCL. EXC. FILL	8,650 CU. YD.
BORROW	6,920 CU. YD.
SHRINKAGE:	
UNCL. EXC. BORROW	20%



CROSS SECTION THRU RDWY. LOOKING NORTH



PROFILE GRADE LINE

TOTAL ESTIMATED QUANTITIES

BID ITEMS	UN. IT	SUPER.	SOUTH ABUT.	NORTH ABUT.	TOTAL
REMOVING OLD BRIDGE, STA 15+50	L.S.	—	—	—	1
EXCAVATION FOR STRUCTURES, BRIDGES B-35-84	L.S.	—	—	—	1
CONCRETE MASONRY, BRIDGES	C.Y.	59	101	109	269
PRESTRESSED GIRDERS, I TYPE, 36-INCH	LF.	227	—	—	227
HIGH STRENGTH BAR STEEL REINFORCEMENT, BRIDGES	LBS.	11,910	8550	10,080	30,540
BEARING PADS, ELASTOMERIC	S.F.	8	—	—	8
TUBULAR RAILING, TYPE "F", STRUCTURE B-35-84	L.S.	—	—	—	1
NON-BID ITEMS					
POLYVINYL CHLORIDE WATERSTOP	LF.	—	75	80	155
FILLER	SIZE	—	—	—	1/2", 3/4"

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B - 35 - 84			
Const. Spec. 1981	Drawn By NJA	Plans Checked RIS	
GENERAL PLAN			SHEET 2 OF 11
			X67853

9411-01-71

ABBREVIATIONS
 F — Fine M — Medium C — Coarse
 Ws — Weathered So — Sound

MATERIAL SYMBOLS
 Topsoil Silt Sandstone
 Sand Peat Limestone
 Gravel Clay Igneous Rock

LEGEND OF PROBING
 Probing No. Sta. Elevation
 95/6=95 Blows for 6" Penetration
 Probing taken with a 350# wt. Falling 18" on a 2" O. D. Point.
 7 Average Blows Per Foot
 Refusal 95/6

LEGEND OF BORING
 Boring No. Sta. Elev.
 Unconfined Strength → 7.7
 Blows Per Ft. Using 140# Wt. Falling 30"
 Wash Sample
 Shelby Tube — S. T.
 Ground Water Elevation
 No Ground Water Observed Above This Elevation
 Sandy Gravel
 Boulders or Cobbles
 Sand
 Silty Clay
 So
 Limestone

Unless otherwise specified, the blows per foot at the locations indicated are based on driving a 2" O. D. x 1.4" I. D. split spoon sampler with a 140# hammer having a free fall of 30". The blow count is taken in undisturbed soil immediately below a cased or open hole eliminating side friction on the drive pipe.

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

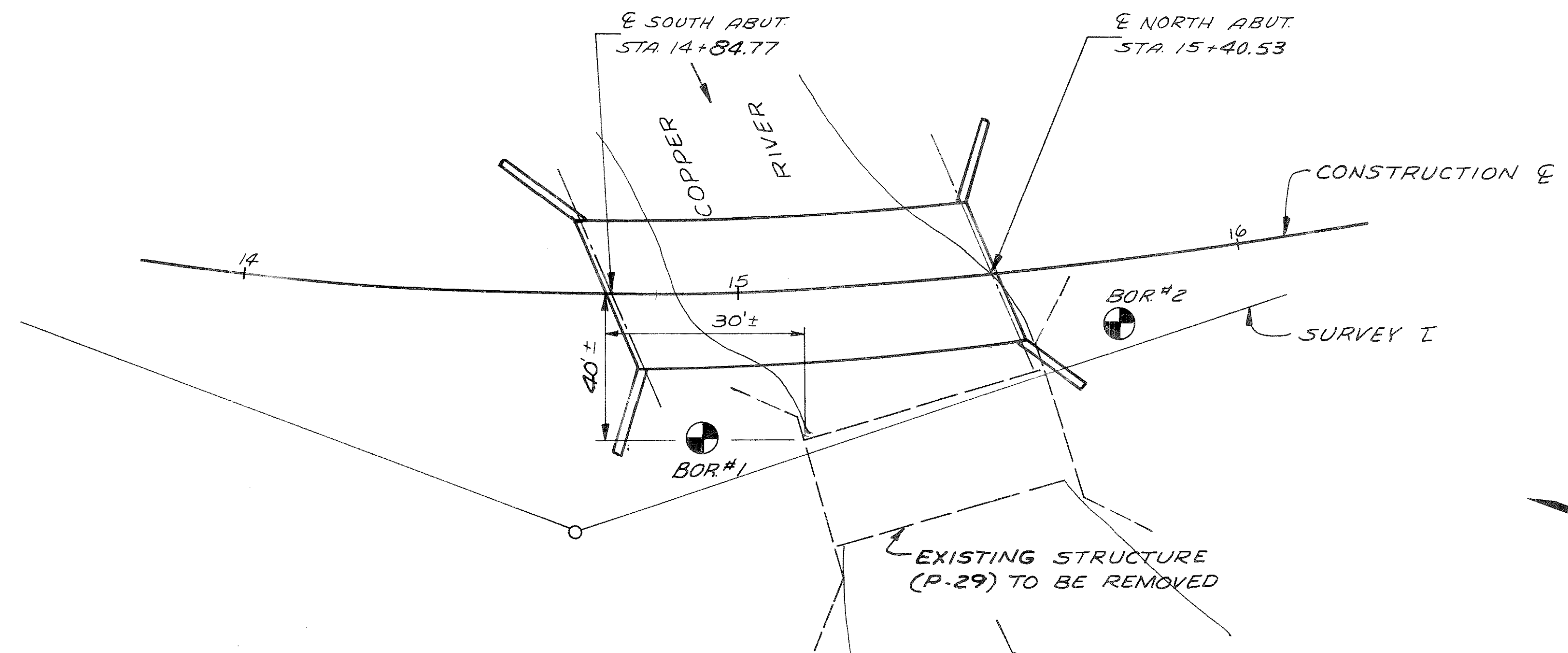
To obtain relative data concerning the character of material in and upon which the foundation might be built, borings and/or soundings were made at points approximately as indicated on this drawing. The data presented herein represents the findings of the subsurface explorations made. However, because the depths investigated are limited and the area of the borings and/or soundings is very small in relation to the entire area, the Division of Highways does not warrant conditions below the depths investigated or that the classification of material encountered in these investigations is necessarily typical of the entire site.

No.	Date.	Revision	By

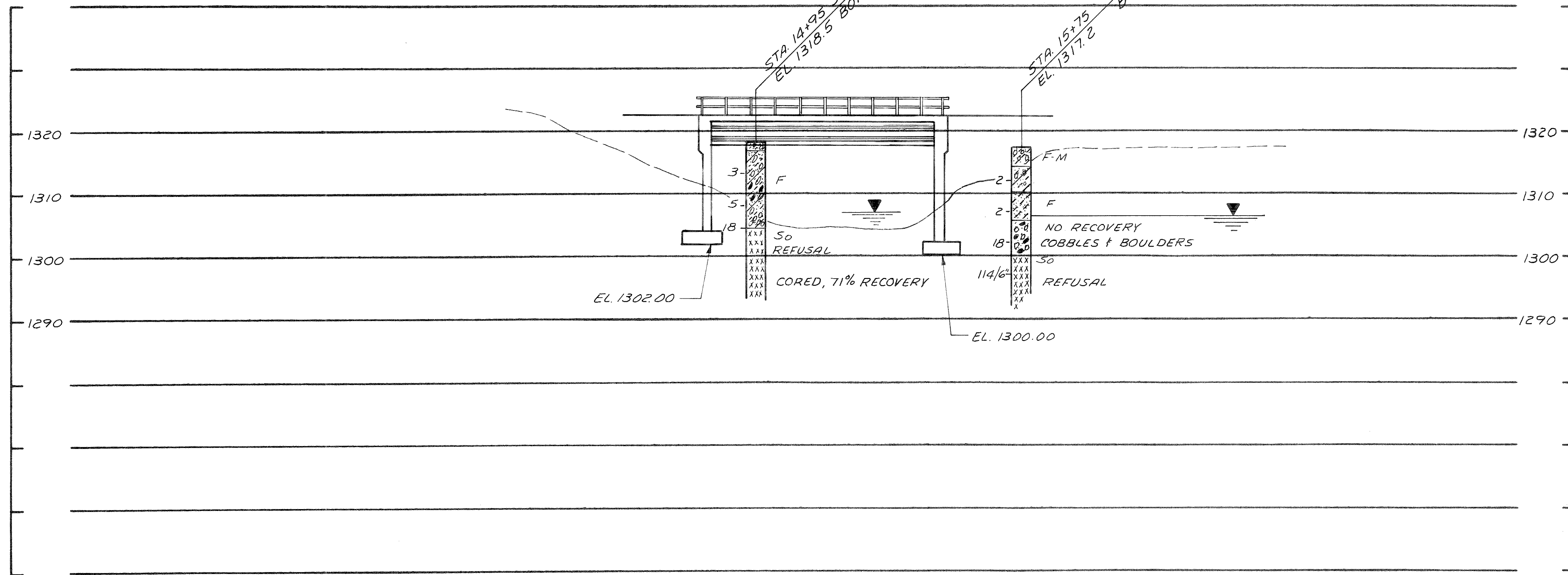
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

STRUCTURE B-35-84
 Const. Spec. 1981 Drawn By NJA Plans Checked RIS

SUBSURFACE EXPLORATION SHEET 3 OF 11
 X67854



BORINGS PERFORMED BY:
SOILS & ENGINEERING SERVICES, INC.
 MADISON, WISCONSIN
 ON JULY 15, 1980

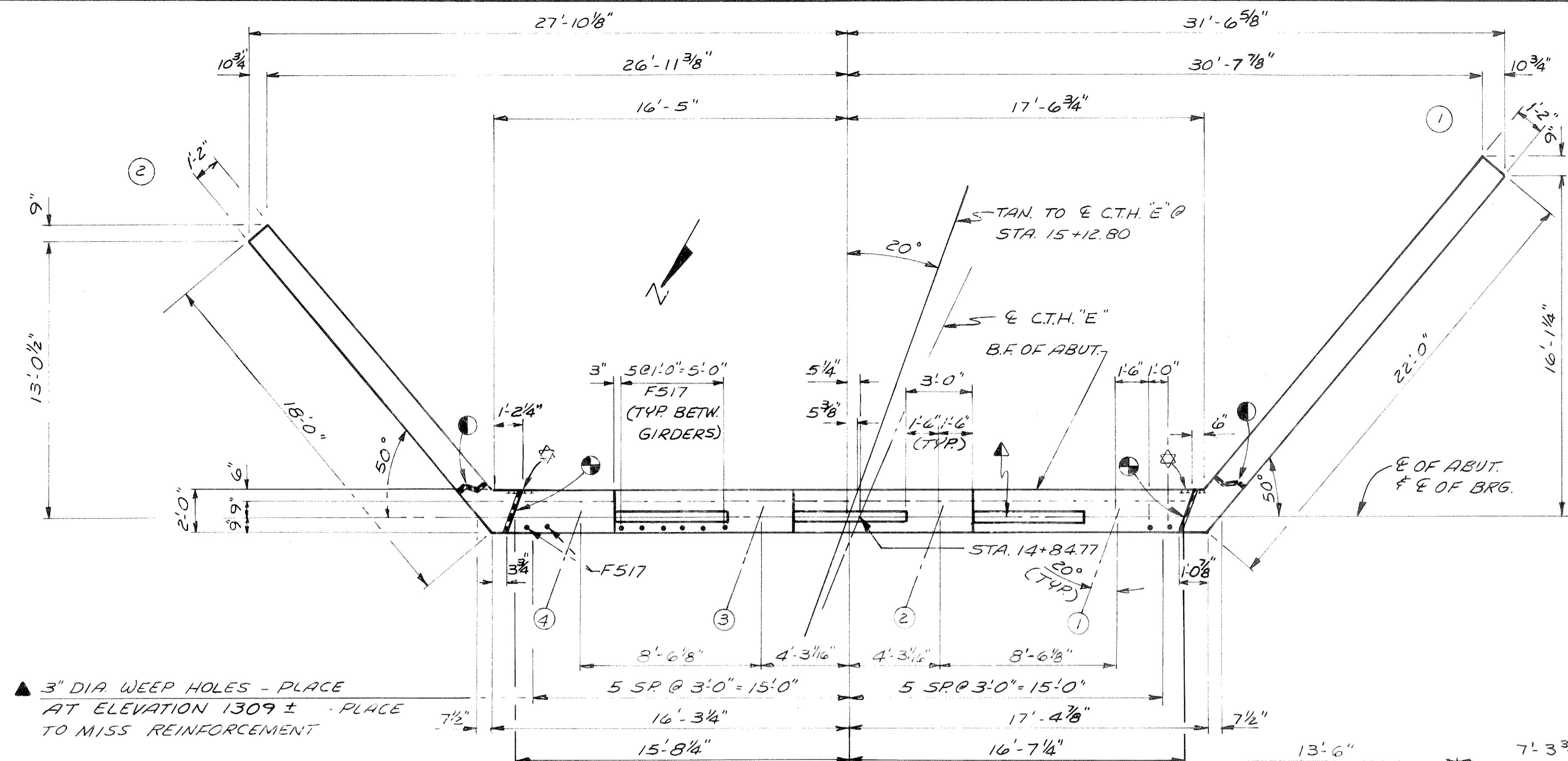


NOTE: F517 BARS MAY BE PLACED AFTER CONC. IS POURED, BUT BEFORE INITIAL SET HAS TAKEN PLACE.

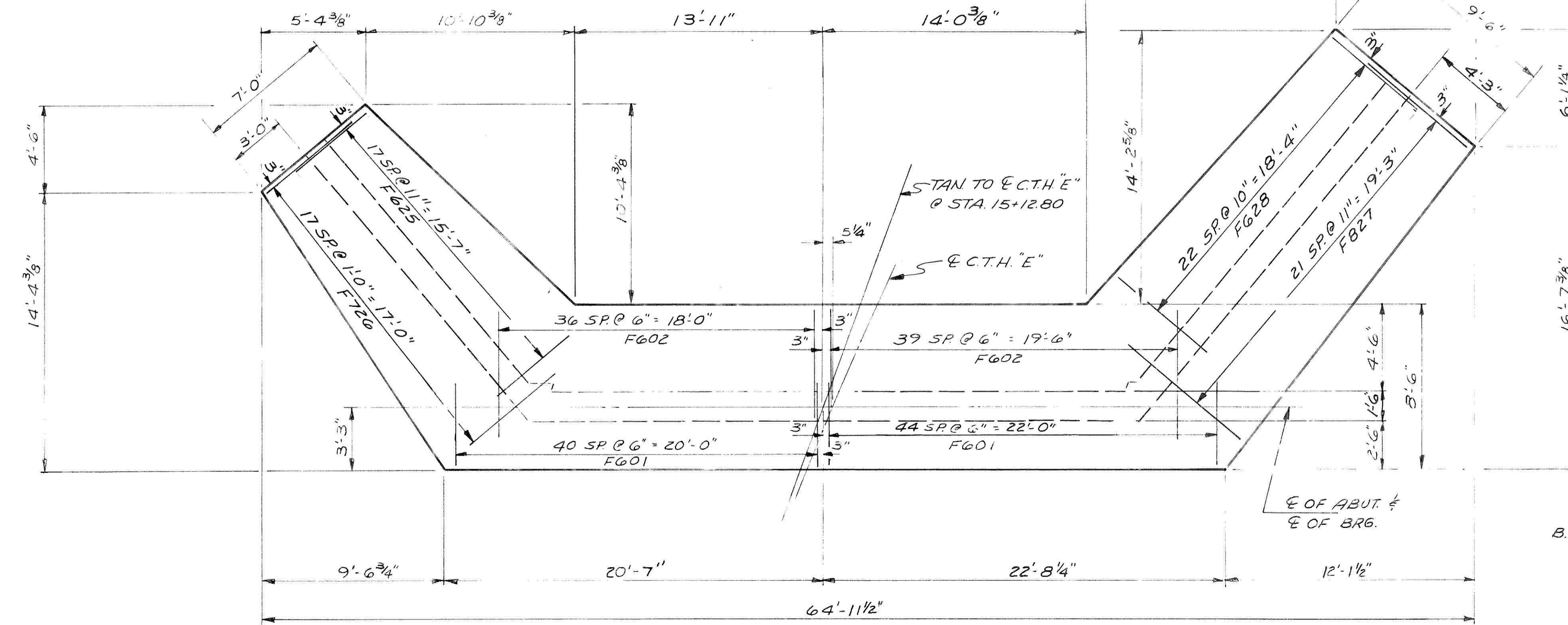
NOTE: ABUTMENT SUPPORTED ON SPREAD FOOTINGS WITH A MIN. ALLOWABLE SOIL BEARING PRESSURE OF 3 TONS PER SQUARE FOOT.

** SEE FOUNDATION DATA SHT. 1.

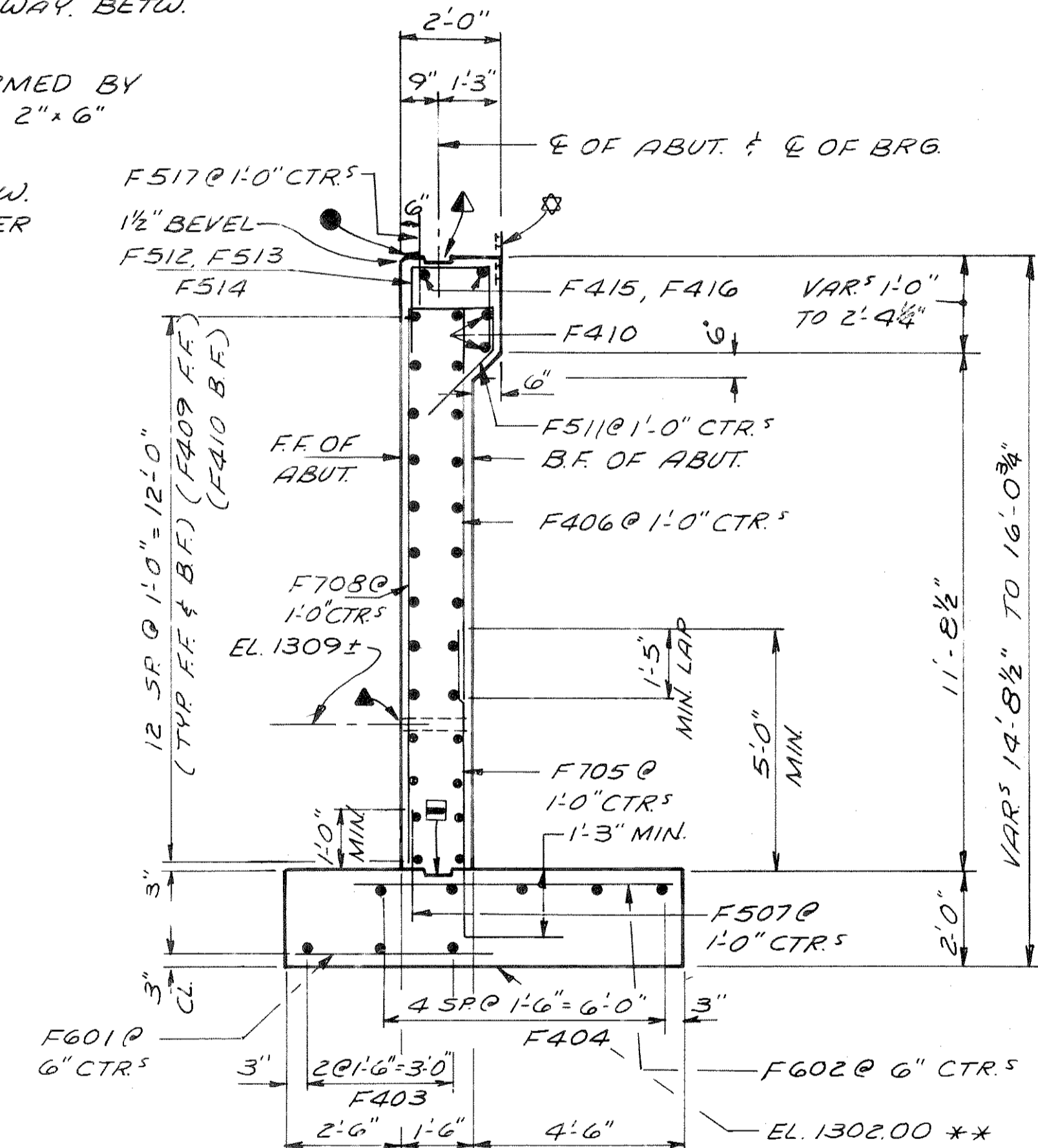
- ① 3/4" FILLER - SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 3/4" FILLER WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER. (1" DEEP & HOLD 1/8" BELOW SURFACE OF CONC.) EXTEND FROM TOP OF FOOTING TO TOP OF WING.
- ② 1/2" FILLER & SEALER. SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP & HOLD 1/8" BELOW SURFACE OF CONC.) EXTEND FROM TOP OF BODY TO TOP OF WING.
- ③ POLYVINYL CHLORIDE WATERSTOP - TO EXTEND BETWEEN FACES OF WINGS, FROM TOP OF FOOTING TO TOP OF WING AND FROM TOP OF BODY TO TOP OF SLAB. (SEE DETAIL SHT. 8)
- ④ CONST. JOINT - FORMED BY SURFACED, BEVELED 2"x6" KEYWAY, BETW. BEAM SEATS.
- ⑤ CONST. JOINT - FORMED BY SURFACED, BEVELED 2"x6" KEYWAY.
- ⑥ 4"x3/4" FILLER - BETW. WINGS EXCEPT UNDER GIRS.



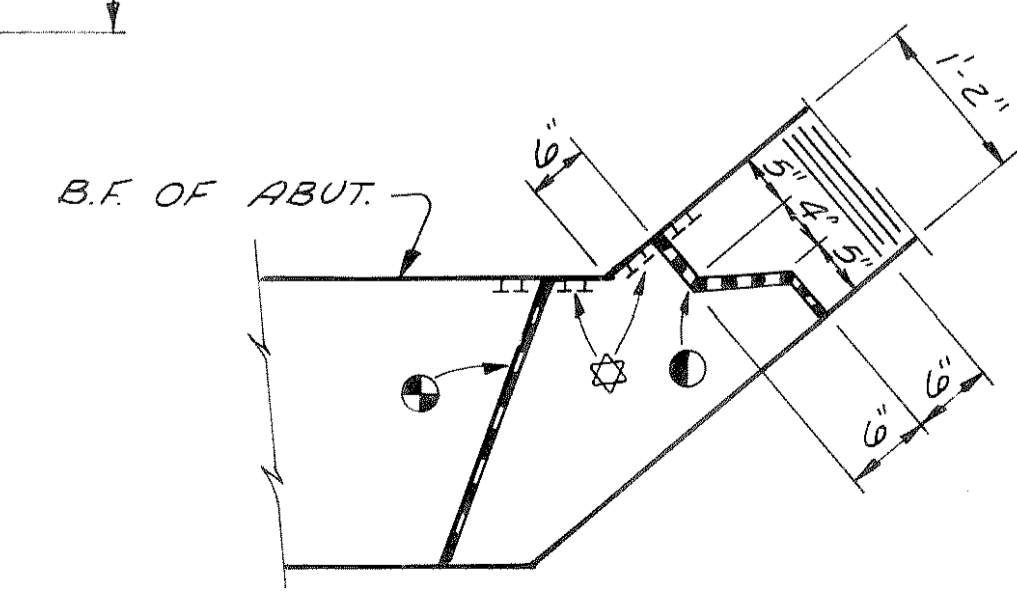
PLAN OF BODY & WINGS



FOOTING PLAN

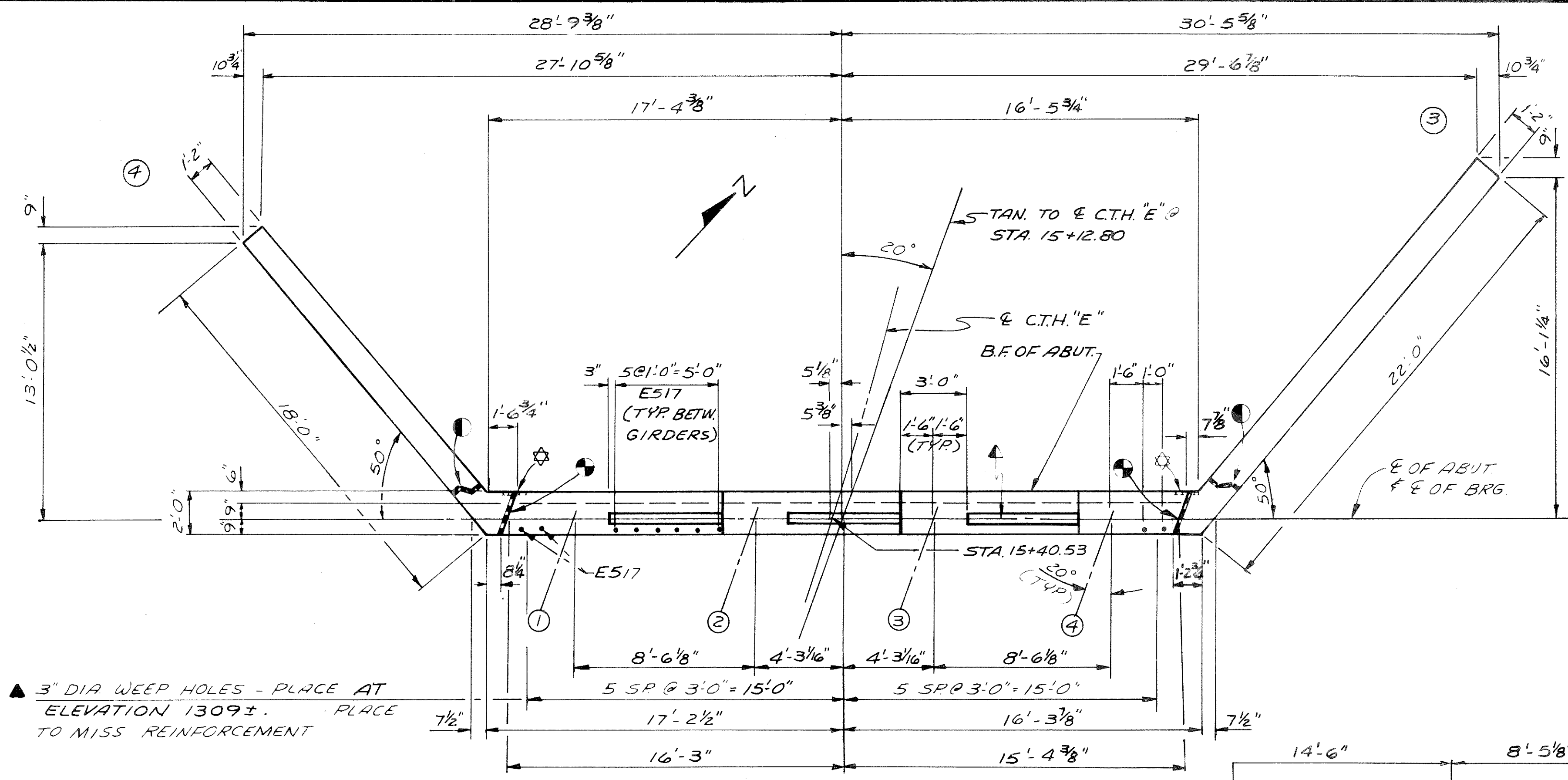


SECTION THRU BODY



JOINT IN WINGS (TYP.)

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B - 35 - 84			
Const. Spec. 1981	Drawn By NJA	Plans Checked RIS	
SOUTH ABUTMENT			SHEET 4 OF 11
X67855			



▲ 3" DIA WEEP HOLES - PLACE AT ELEVATION 1309±. PLACE TO MISS REINFORCEMENT

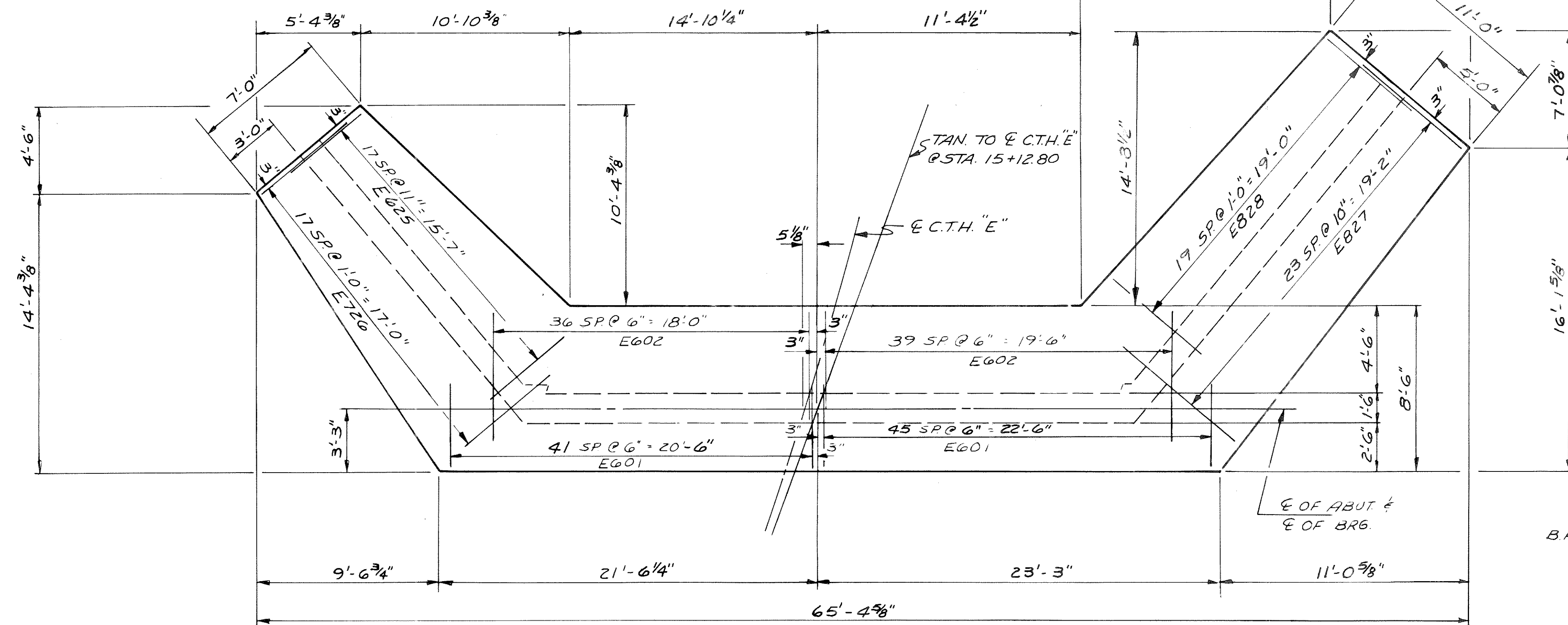
- 3/4" FILLER - SEAL ALL EXPOSED HORIZ & VERT SURFACES OF 3/4" FILLER WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER. (1" DEEP & HOLD 1/8" BELOW SURFACE OF CONC.) EXTEND FROM TOP OF FOOTING TO TOP OF WING.
- 1/2" FILLER & SEALER SEAL ALL EXPOSED HORIZ & VERT SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER. (1" DEEP & HOLD 1/8" BELOW SURFACE OF CONC.) EXTEND FROM TOP OF BODY TO TOP OF WING.
- POLYVINYL CHLORIDE WATERSTOP - TO EXTEND BETWEEN FACES OF WINGS, FROM TOP OF FOOTING TO TOP OF WING AND FROM TOP OF BODY TO TOP OF SLAB. (SEE DETAIL SHT. 8)
- ▲ CONST. JOINT - FORMED BY SURFACED, BEVELED 2"x6" KEYWAY, BETW. BEAM SEATS.
- CONST. JOINT - FORMED BY SURFACED, BEVELED 2"x6" KEYWAY.
- 4"x3/4" FILLER - BETW. WINGS EXCEPT UNDER GIRTS

NOTE: E517 BARS MAY BE PLACED AFTER CONC IS POURED, BUT BEFORE INITIAL SET HAS TAKEN PLACE

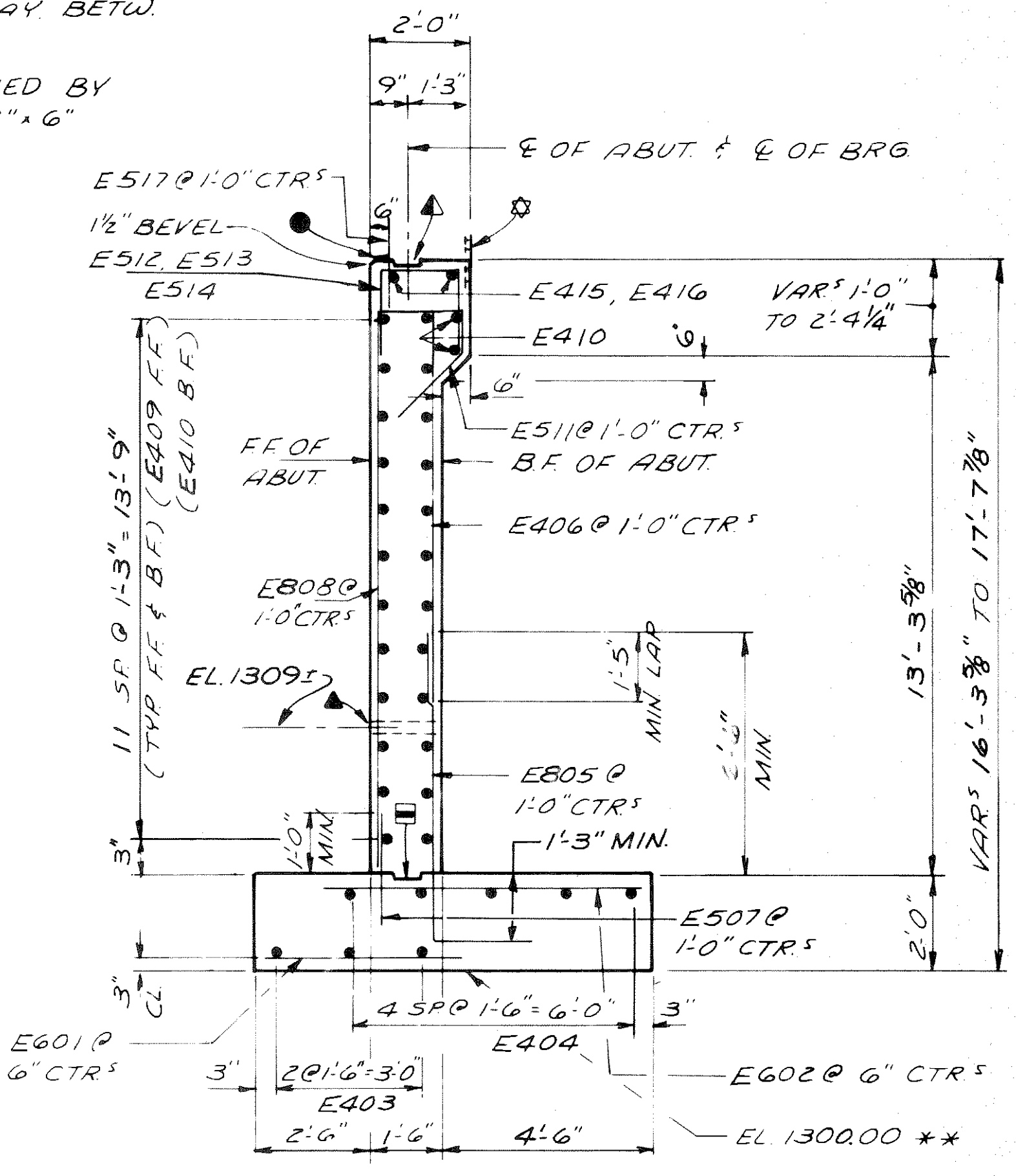
NOTE: ABUTMENT SUPPORTED ON SPREAD FOOTINGS WITH A MIN ALLOWABLE SOIL BEARING PRESSURE OF 3 TONS PER SQUARE FOOT

** SEE FOUNDATION DATA SHT. 1

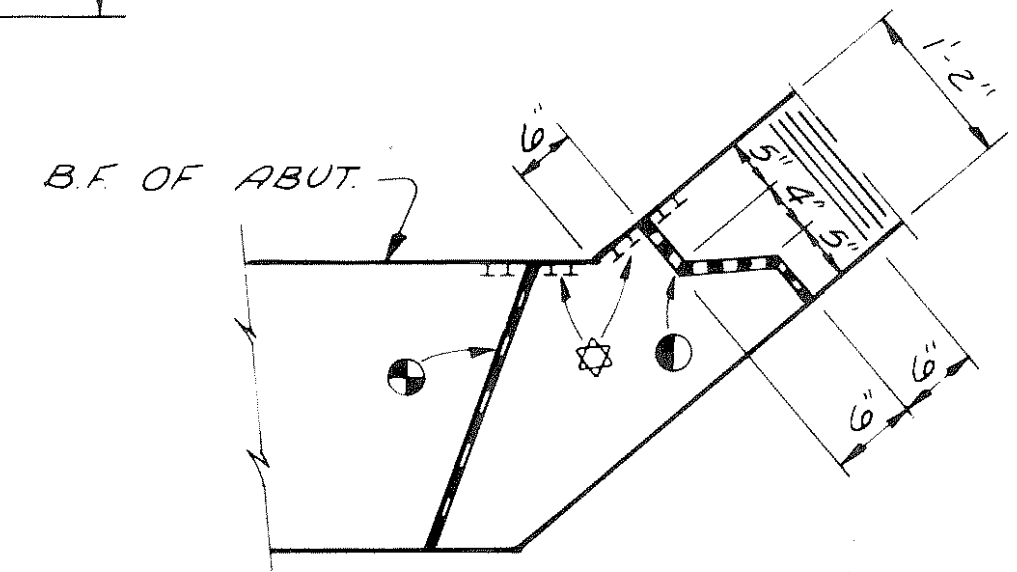
PLAN OF BODY & WINGS



FOOTING PLAN

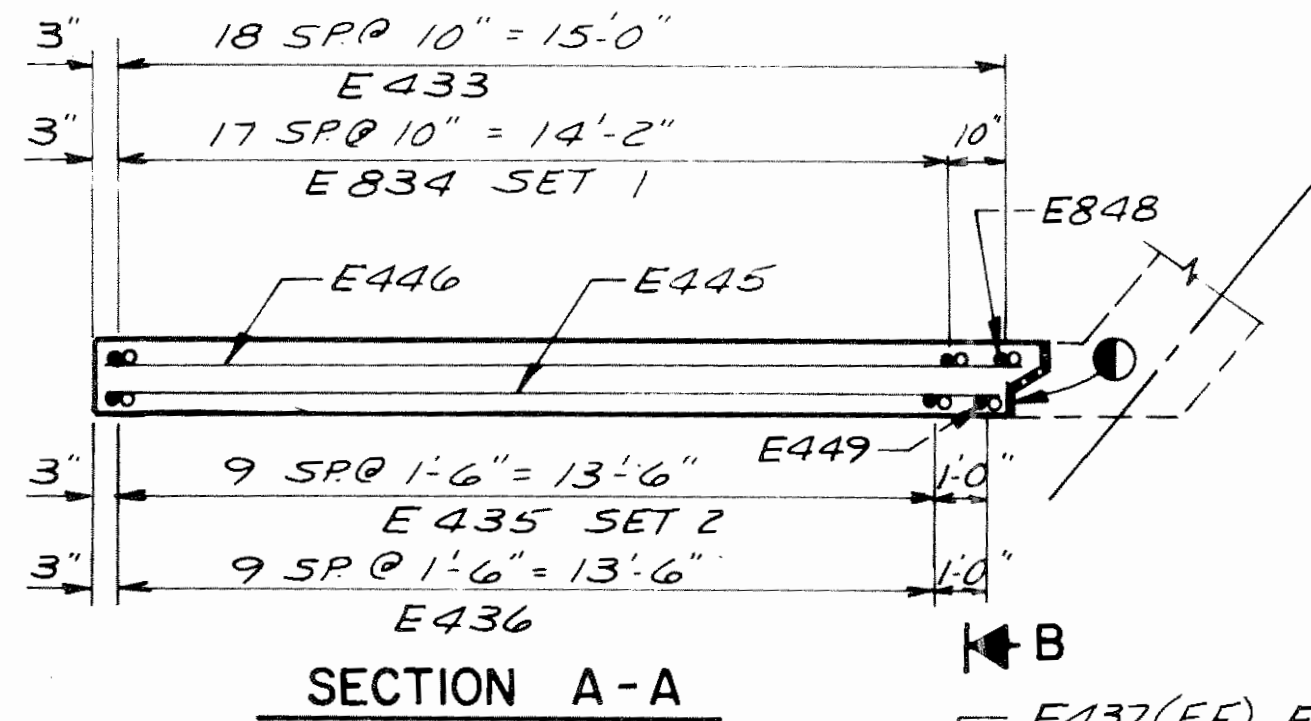


SECTION THRU BODY

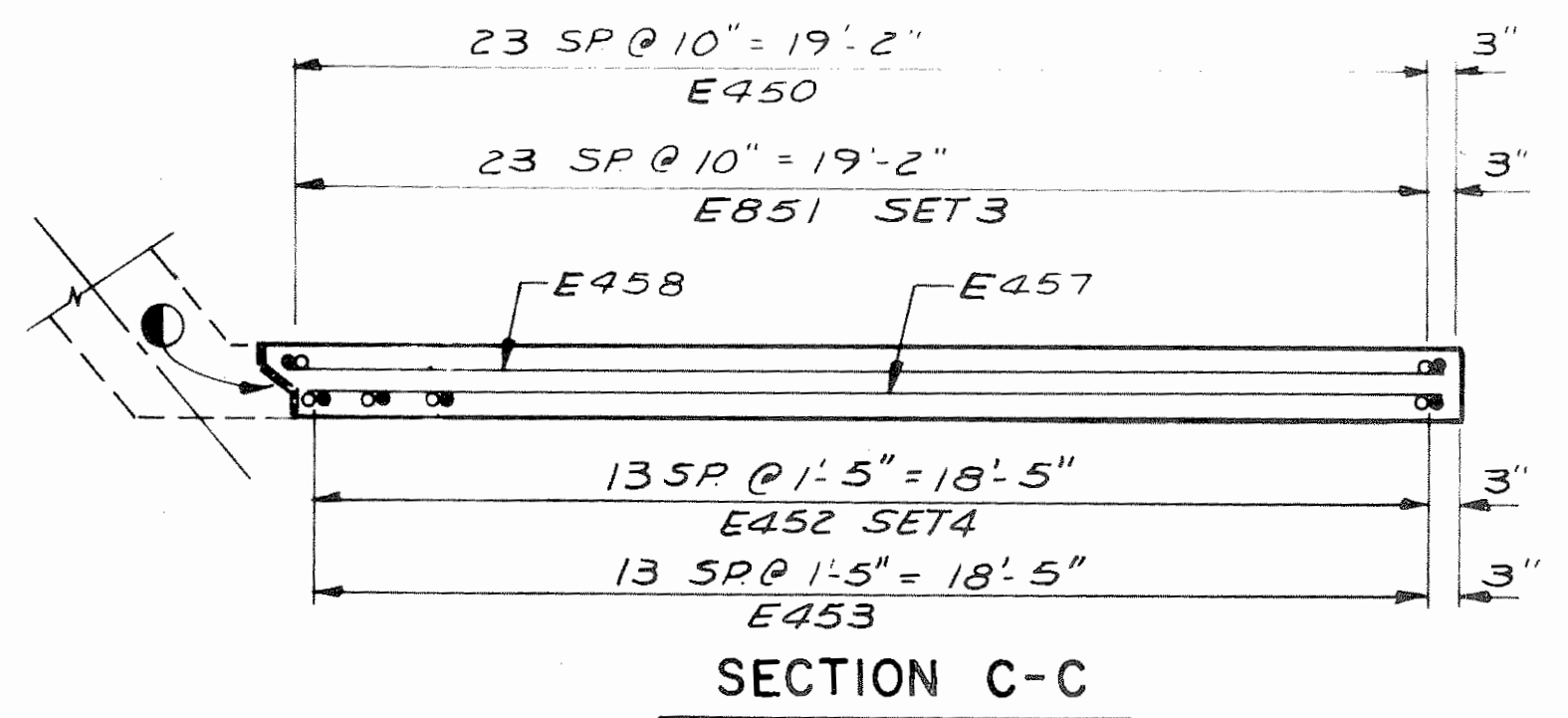


JOINT IN WINGS (TYP)

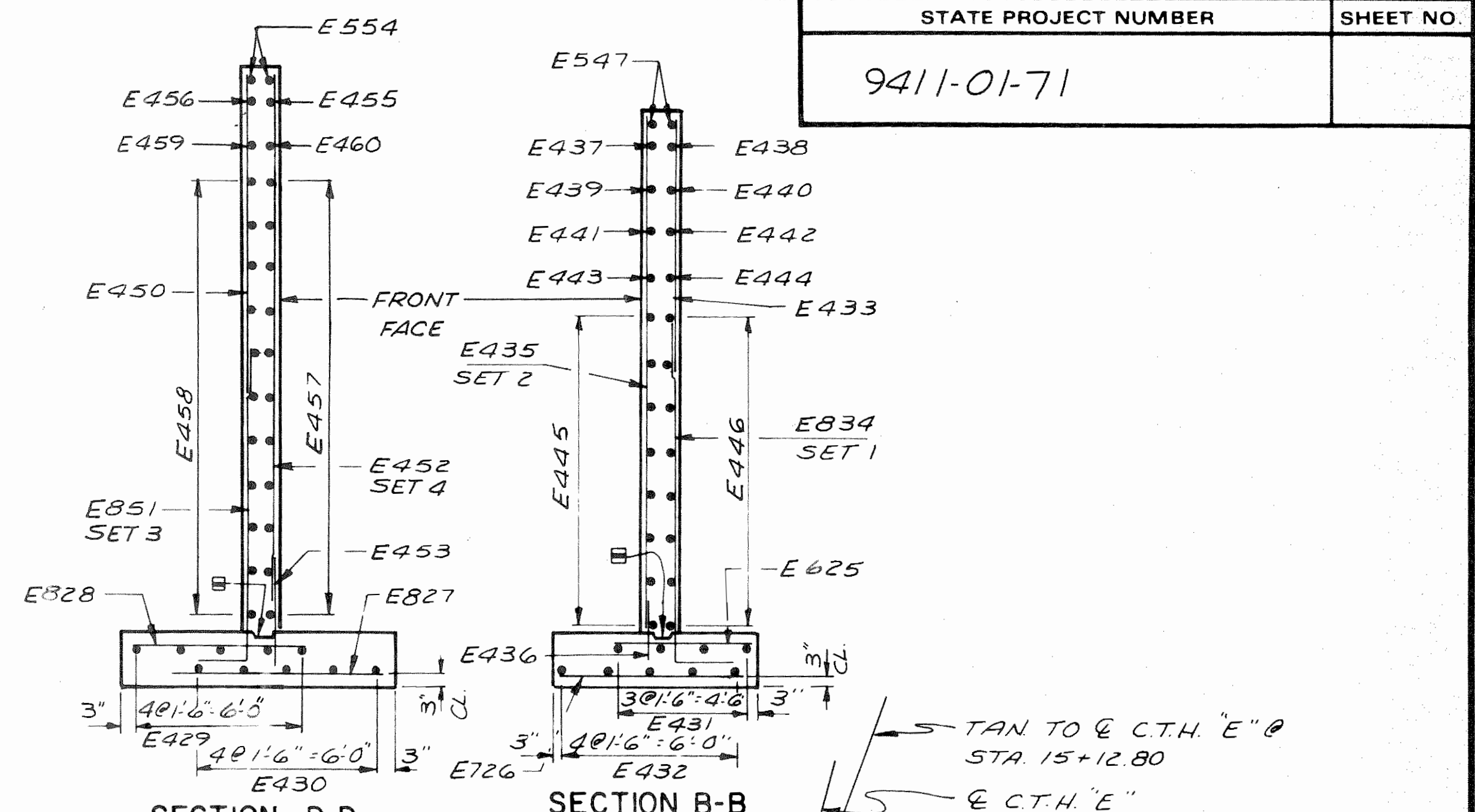
No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-35-84			
Const. Spec. 1981	Drawn By NJA	Plans Checked RIS	
NORTH ABUTMENT			SHEET 6 OF 11 X67857



SECTION A-A

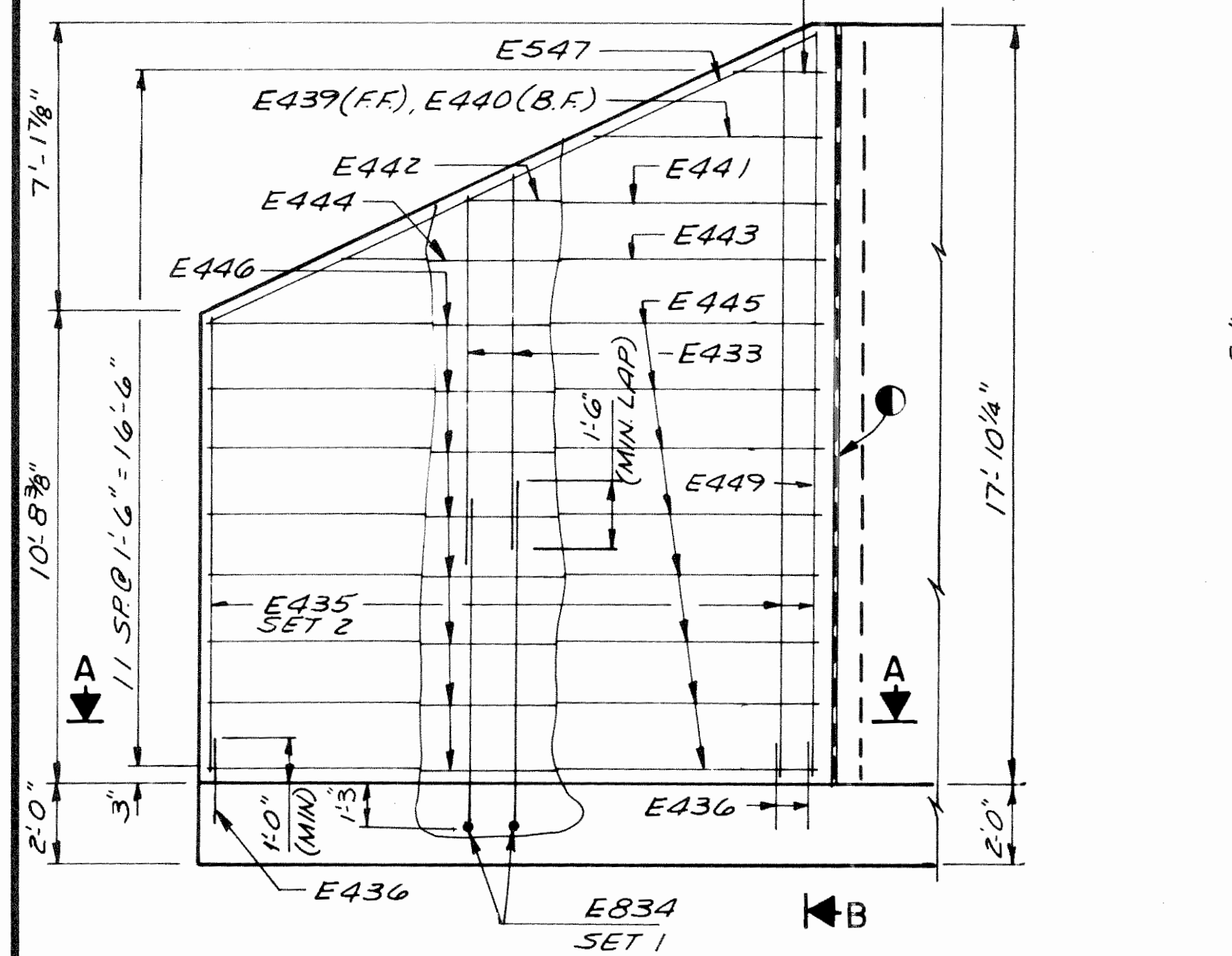


SECTION C-C

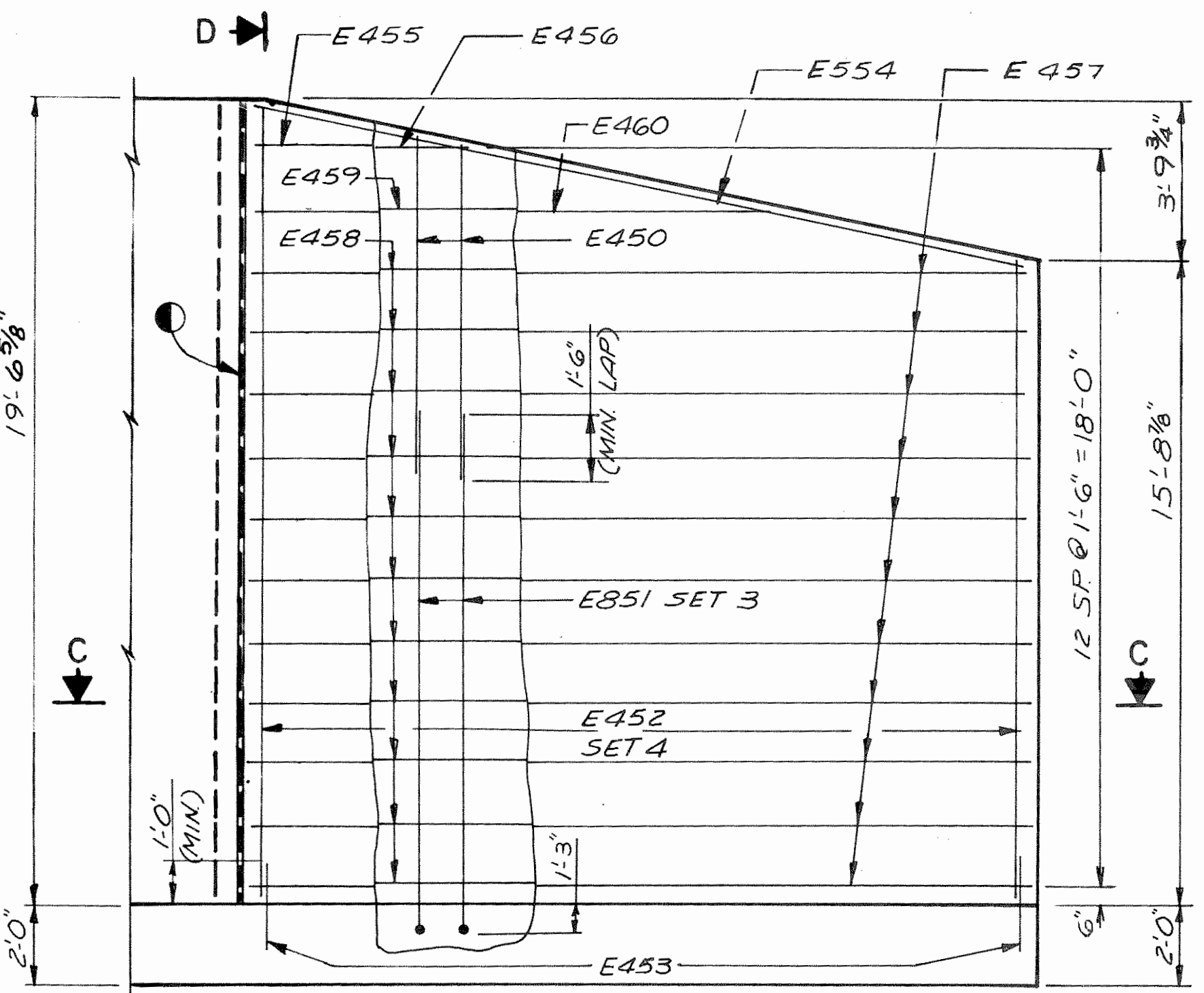


SECTION D-D

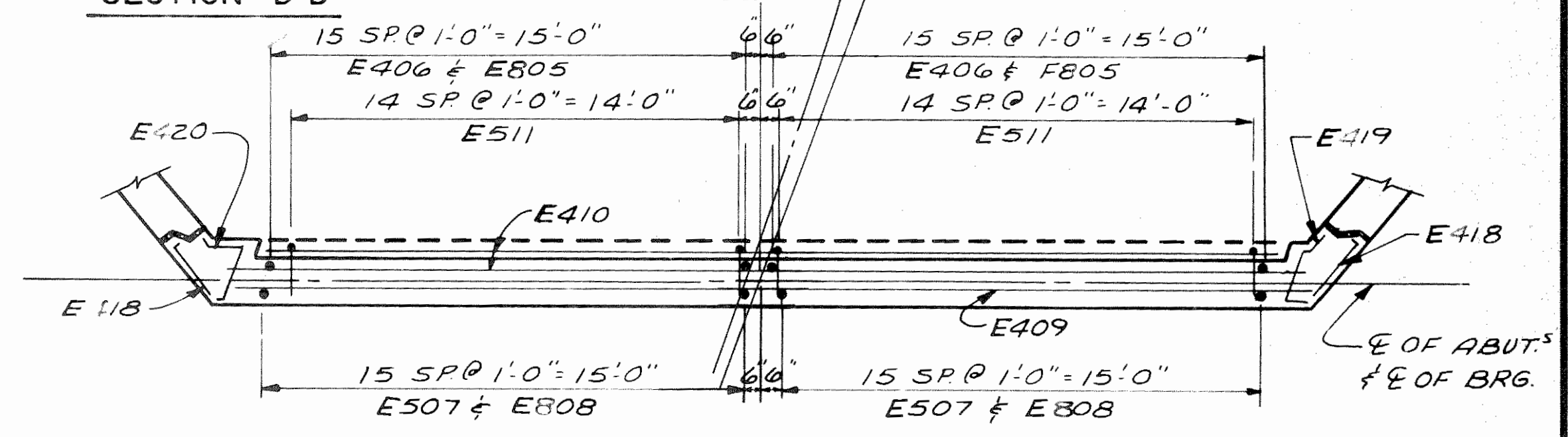
SECTION B-B



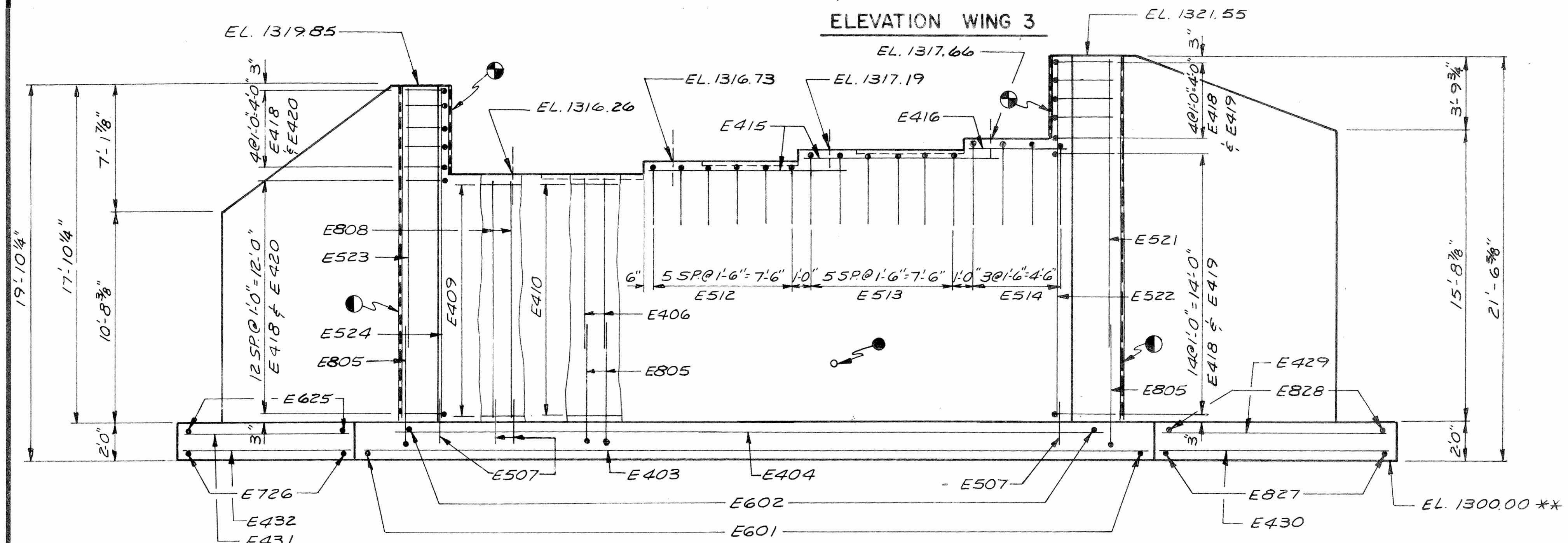
ELEVATION WING 4



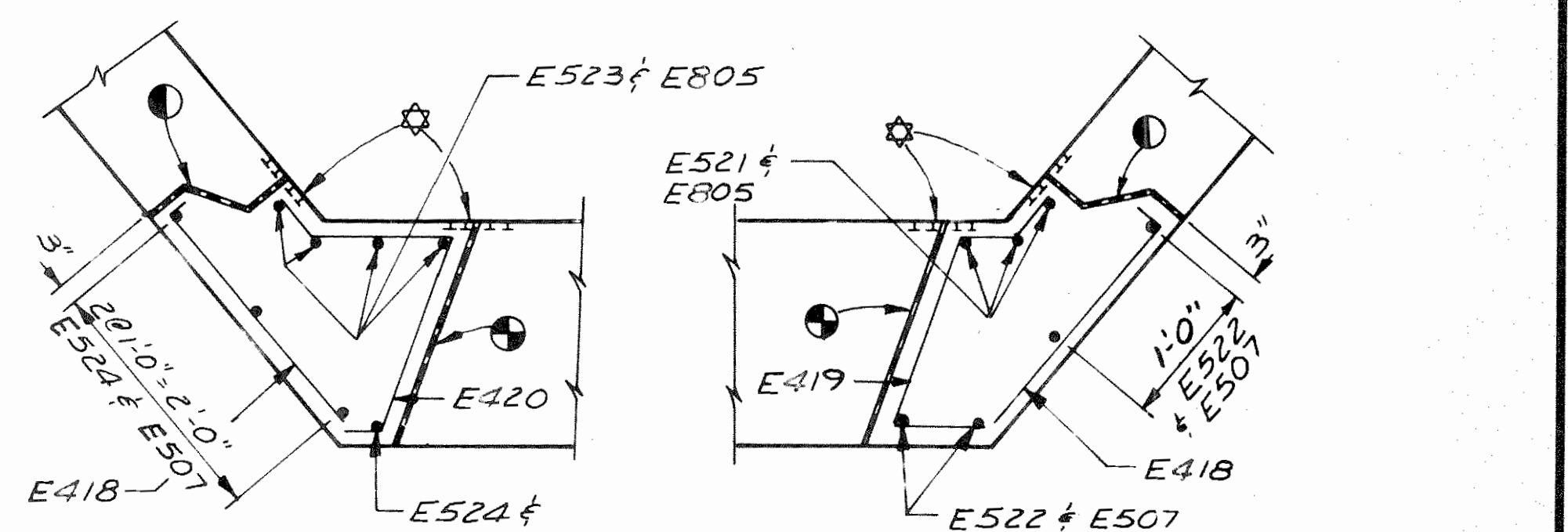
ELEVATION WING 3



PLAN SHOWING BODY REINF.



ELEVATION



PLAN AT WINGS

- ** SEE FOUNDATION NOTE SHT. 1.
- 3" DIA WEEP HOLES - SEE SHT. 6 FOR LOCATION
- ⊙ 1/2" FILLER - SEE NOTE SHT. 6.
- ⊙ 3/4" FILLER - SEE NOTE SHT. 6.
- ▣ CONST. JOINT - FORMED BY SURFACED BEVELED 2" x 6" KEYWAY.
- ⊙ POLYVINYL CHLORIDE WATERSTOP SEE DETAILS SHTS 4 & 8.

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-35-84			
Const. Spec. 1981	Drawn By NJA	Plans Checked RIS	
NORTH ABUTMENT			SHEET 7 OF 11
			X67858

BILL OF BARS (SOUTH ABUT.)

MARK	NO. REQ'D	LENGTH	BENT	CUT DIAG	LOCATION
F601	86	4-3			FOOTING BODY TOE
F602	77	6-9			" " HEEL
F403	3	43-0			" " TOE
F404	5	38-0			" " HEEL
F705	39	9-7	*		BODY B.F. DOWELS VERT.
F406	32	8-9			" " "
F507	40	4-0			" F.F. DOWELS "
F708	32	12-2			" " "
F409	13	33-3			" " HORIZ.
F410	15	33-9			" B.F. "
F511	30	4-4	*		" CORBEL "
F512	6	3-10	*		" TOP VERT.
F513	6	4-10	*		" " "
F514	4	5-10	*		" " "
F415	4	10-0			" " HORIZ.
F416	2	4-6			" " "
F517	22	2-0			" " VERT.
F418	35	2-4	*		" @ WINGS 1&2 HORIZ.
F419	17	3-3	*		" @ " 1 "
F420	18	3-6	*		" @ " 2 "
F521	3	12-5			" @ " 1 VERT.
F522	4	15-10			" @ " 1 "
F523	4	14-2			" @ " 2 "
F524	4	17-6			" @ " 2 "
F625	18	6-0			FOOTING WING 2 HEEL
F726	18	6-0			" " 2 TOE
F827	22	6-11			" " 1 "
F628	23	7-0			" " 1 HEEL
F429	4	19-0			" " 1 "
F430	5	19-6			" " 1 TOE
F431	4	15-9			" " 2 HEEL
F432	5	17-3			" " 2 TOE
F433	19	7-6			WING 2 BF VERT.
F834	9	25-10	*	*	" " " "
F435	5	28-5		*	" " FF "
F436	11	4-0			" " " "
F437	1	2-3			" " " HORIZ.
F438	1	2-9			" " BF "
F439	1	5-6			" " FF "
F440	1	6-0			" " BF "
F441	1	8-9			" " FF "
F442	1	9-3			" " BF "
F443	1	12-0			" " FF "
F444	1	12-6			" " BF "
F445	8	14-9			" " FF "
F446	8	15-3			" " BF "
F547	2	16-0			" " TOP "
F848	1	15-11			" " BF VERT.
F449	1	17-7			" " FF "
F450	24	9-6			" 1 BF "
F851	24	12-3	*	*	" " " "
F452	14	14-2			" " FF "
F453	14	6-0			" " " "
F554	2	19-1			" " TOP HORIZ.
F455	1	4-3			" " FF "
F456	1	4-9			" " BF "
F457	10	19-0			" " FF "
F458	10	19-6			" " BF "

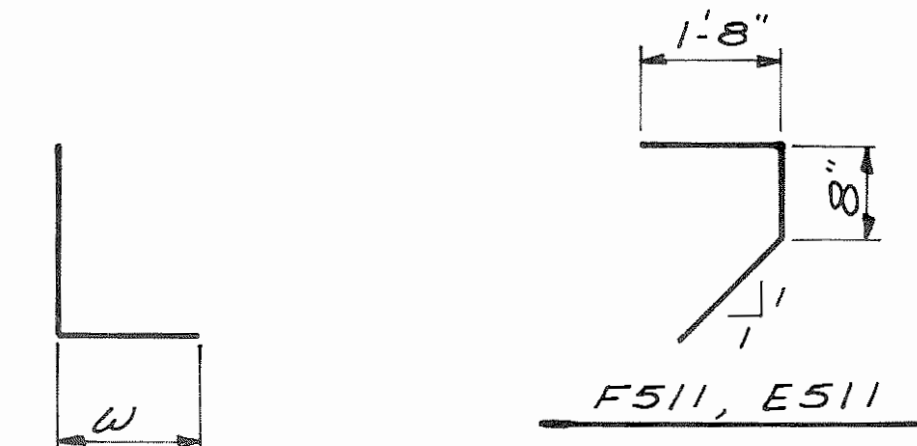
BILL OF BARS (NORTH ABUT.)

MARK	NO. REQ'D	LENGTH	BENT	CUT DIAG	LOCATION
E601	88	4-3			FOOTING BODY TOE
E602	77	6-9			" " HEEL
E403	3	43-0			" " TOE
E404	5	38-0			" " HEEL
E805	39	11-10	*		BODY B.F. DOWELS VERT.
E406	32	9-5			" " "
E507	40	4-0			" F.F. DOWELS "
E808	32	13-10			" " "
E409	12	33-0			" " HORIZ.
E410	14	33-9			" B.F. "
E511	30	4-4	*		" CORBEL "
E512	6	3-10	*		" TOP VERT.
E513	6	4-10	*		" " "
E514	4	5-10	*		" " "
E415	4	10-0			" " HORIZ.
E416	2	4-6			" " "
E517	22	2-0			" " VERT.
E418	38	2-4	*		" @ WINGS 3&4 HORIZ.
E419	20	3-6	*		" @ " 3 "
E420	18	4-0	*		" @ " 4 "
E521	3	15-6			" @ " 3 VERT.
E522	4	19-2			" @ " 3 "
E523	4	13-9			" @ " 4 "
E524	4	17-4			" @ " 4 "
E625	18	6-0			FOOTING WING 4 HEEL
E726	18	6-0			" " 4 TOE
E827	24	9-6			" " 3 "
E828	20	10-0			" " 3 HEEL
E429	5	19-0			" " 3 "
E430	5	19-6			" " 3 TOE
E431	4	15-9			" " 4 HEEL
E432	5	17-3			" " 4 TOE
E433	19	7-6			WING 4 BF VERT.
E834	9	25-10	*	*	" " " "
E435	5	27-7		*	" " FF "
E436	11	4-0			" " " "
E437	1	2-0			" " " HORIZ.
E438	1	2-6			" " BF "
E439	1	5-5			" " FF "
E440	1	5-11			" " BF "
E441	1	8-5			" " FF "
E442	1	8-11			" " BF "
E443	1	11-5			" " FF "
E444	1	11-11			" " BF "
E445	8	14-9			" " FF "
E446	8	15-3			" " BF "
E547	2	16-0			" " TOP "
E848	1	15-11	*		" " BF VERT.
E449	1	17-4			" " FF "
E450	24	9-6			" 3 BF "
E851	12	27-7	*	*	" " " "
E452	7	34-7	*	*	" " FF "
E453	14	4-0			" " " "
E554	2	19-4			" " TOP HORIZ.
E455	1	5-3			" " FF "
E456	1	5-9			" " BF "
E457	11	19-0			" " FF "
E458	11	19-6			" " BF "
E459	1	13-0			" " " "
E460	1	12-6			" " FF "

9411-01-71

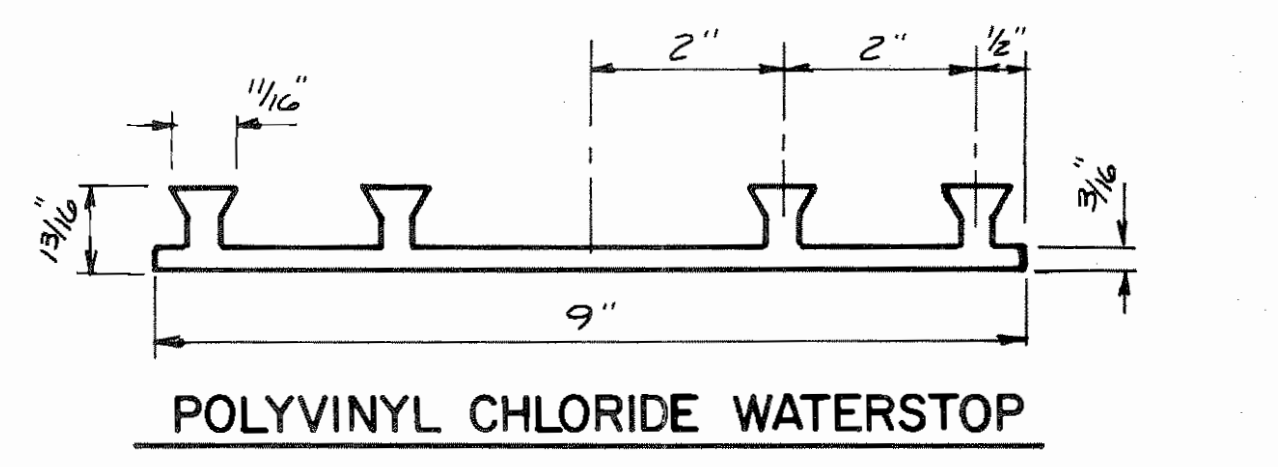
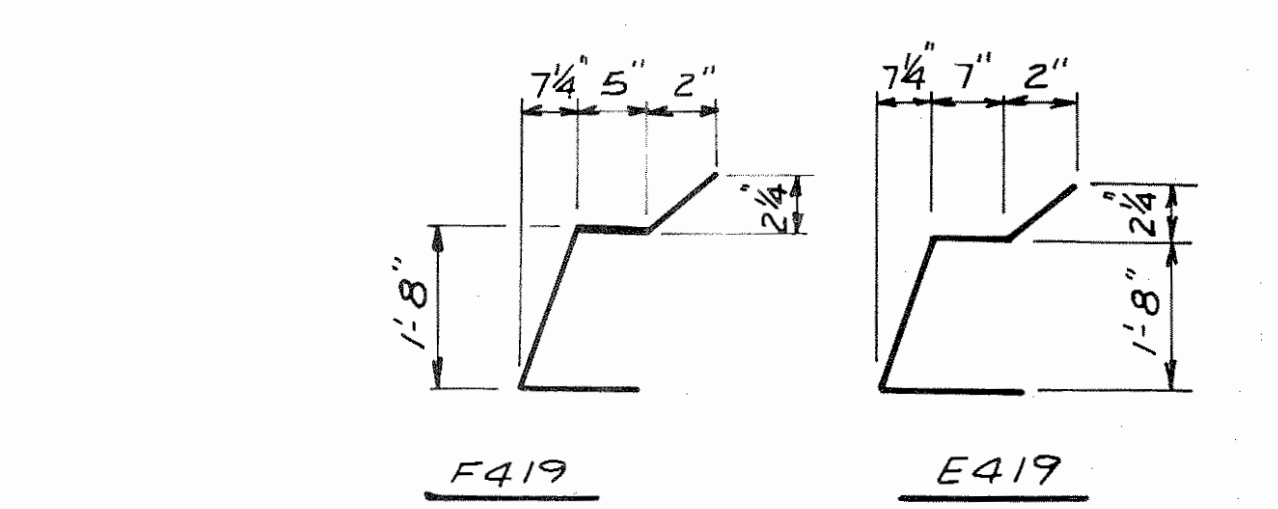
NOTES

BENDING DIMS ARE OUT TO OUT OF BAR.
 THE FIRST DIGIT OF THE BAR MARK SIGNIFIES THE BAR SIZE.
 BENT BARS USED IN CUTTING DIAGRAM SHALL BE BENT AFTER CUTTING.



MARK	W
F705	1'-0"
F418	3"
F834	1'-0"
F848	1'-0"
F851	1'-0"
E805	1'-4"
E418	3"
E834	1'-0"
E851	1'-0"
E848	1'-0"

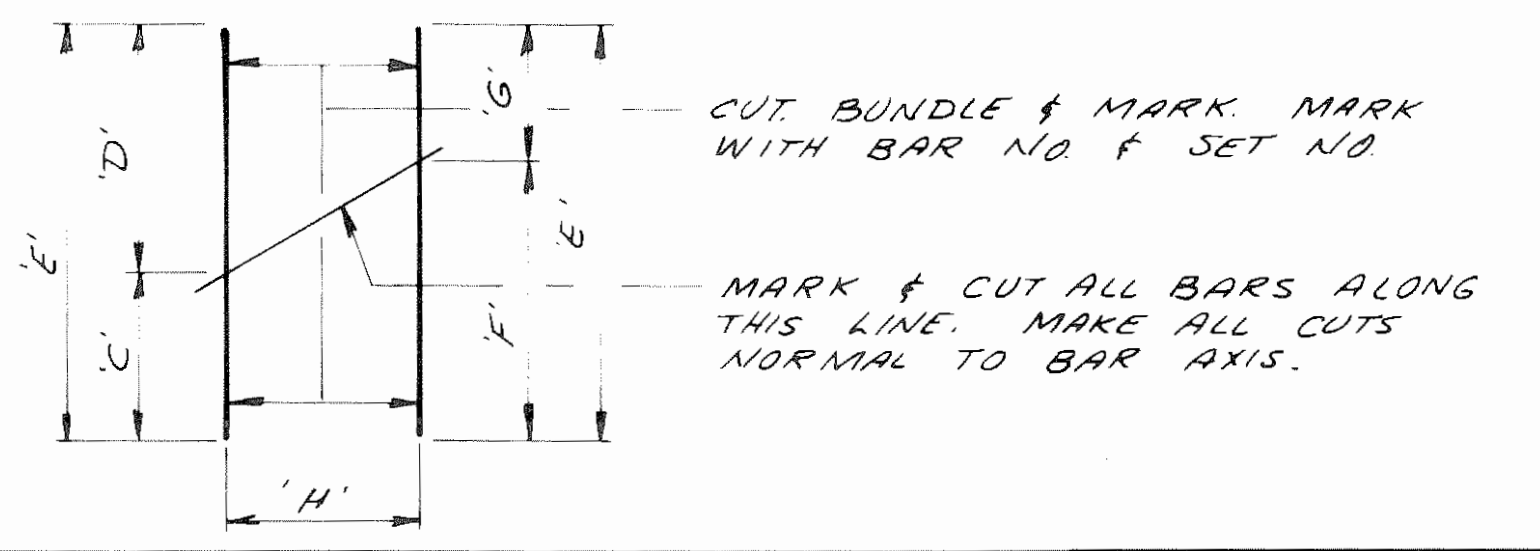
MARK	A	B
F512	1'-8"	1'-2"
F513	1'-8"	1'-8"
F514	1'-8"	2'-2"
E512	1'-8"	1'-2"
E513	1'-8"	1'-8"
E514	1'-8"	2'-2"



CUTTING DIAGRAM

H IS NO OF BARS REQ'D BEFORE CUTTING.

MARK	SET NO.	C	D	E	F	G	H	SETS REQ'D.
F834	1	9-6	16-4	25-10	12-8	13-2	9	1
F435	2	11-0	17-5	28-5	13-10	14-7	5	1
E834	1	9-6	16-4	25-10	12-8	13-2	9	1
E435	2	10-7	17-0	27-7	13-5	14-2	5	1
E851	3	11-10	15-9	27-7	13-8	13-11	12	1
E452	4	15-4	19-3	34-7	17-2	17-5	7	1

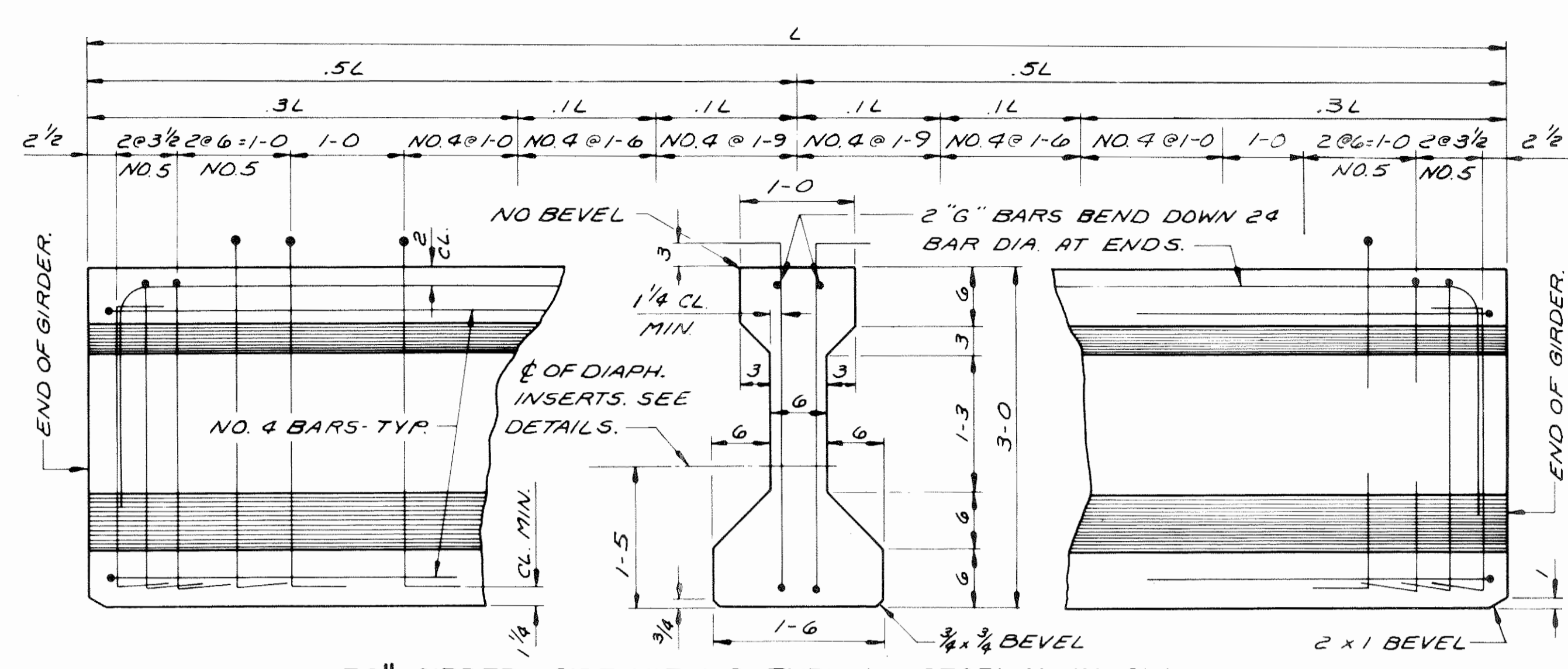


No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-35-84			
Const. Spec. 1981	Drawn By NJA	Plans Checked RIS	
ABUTMENT BILL OF BARS			SHEET 8 OF 11 X67859

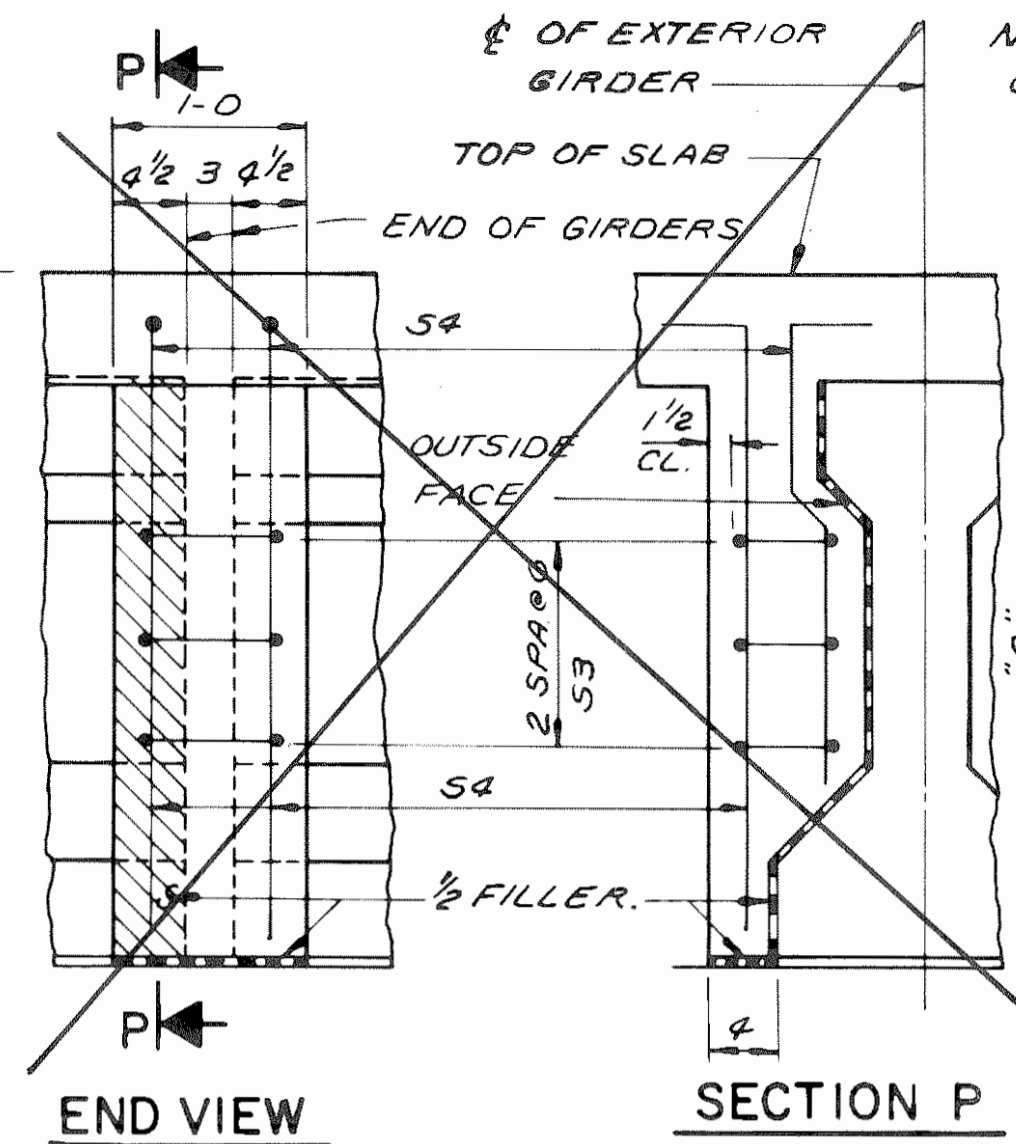
9411-01-71

NOTES

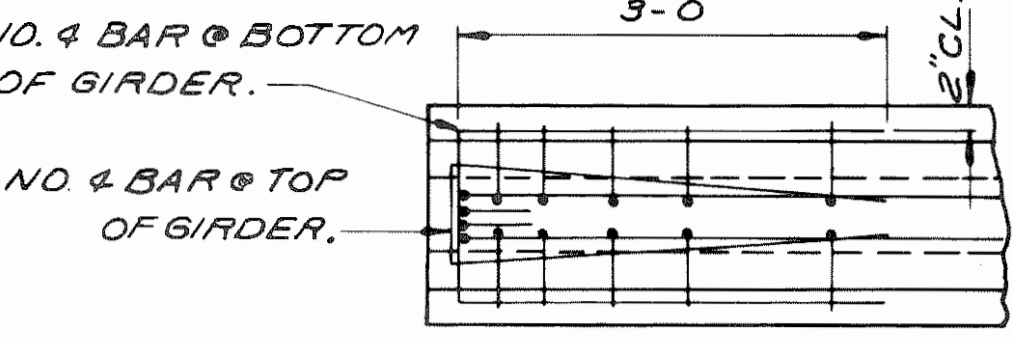
TOP OF GIRDERS TO BE ROUGH FLOATED AND BROOMED TRANSVERSELY FOR BONDING TO SLAB. THE GIRDERS SHALL BE PROVIDED WITH A SUITABLE LIFTING DEVICE FOR HANDLING AND ERECTING THE GIRS. ALL GIRDERS SHALL BE CAST FULL LENGTH AS SHOWN. PRESTRESSING STRANDS SHALL BE 1/2" φ-7 WIRE STRANDS WITH AN ULTIMATE STRENGTH OF 270,000 PSI. AND SHALL BE FLUSH WITH THE ENDS OF THE GIRDER. INSERTS SHALL BE PLACED ON 4" CENTERS SYMMETRICALLY ABOUT THE φ OF DIAPHRAGMS IN SPANS. ALL STIRRUPS SHALL BE IN PAIRS AND THE SPACING SHOWN IN "SIDE VIEW" IS MAXIMUM. THE LOCATION SHALL BE SHOWN IN THE SHOP DRAWINGS. BEND EACH END OF NO. 4 AND NO. 5 STIRRUPS 60° AND NO. 6 STIRRUPS 90°. ENDS OF STRANDS SHALL BE PAINTED WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (THIS APPLIES ONLY TO THOSE ENDS OF GIRDERS THAT ARE FINALLY EXPOSED.) BARS "G" MAY BE SPliced AT THE 1/3 POINTS OF GIRDER. USE LAP LENGTH SHOWN IN "GIRDER DATA". DEFLECTION DATA IS THEORETICAL AND MAY VARY WITH CONCRETE STRENGTH, VARIABLE PRESTRESS CONDITIONS AND PRESTRESS LOSSES.



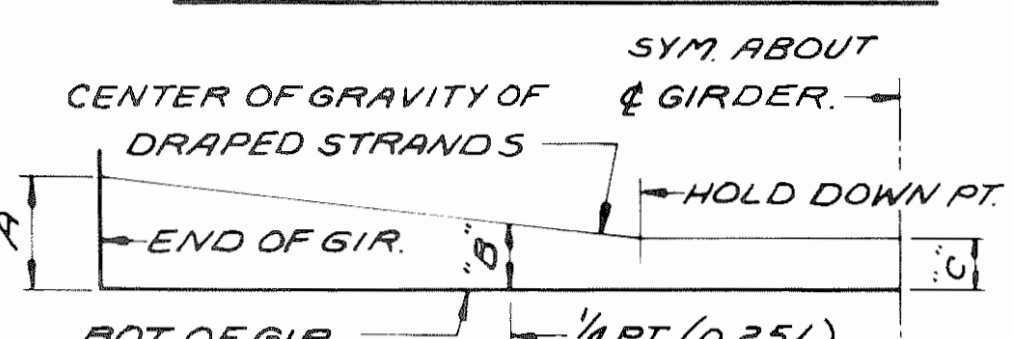
36" GIRDER - SIDE VIEW & TYPICAL SECTION IN SPAN



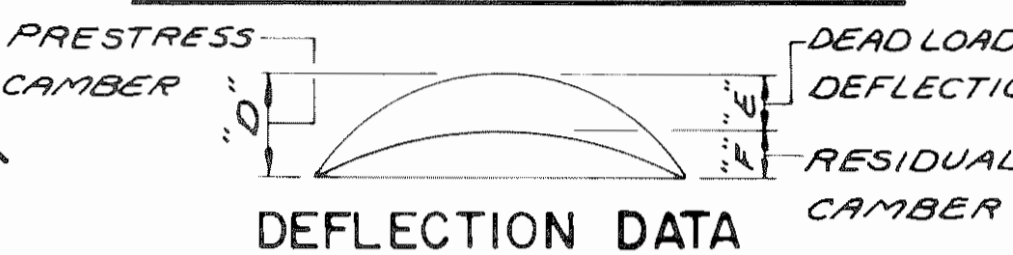
PILASTER DETAIL AT PIERS



TOP VIEW OF GIRDER ENDS



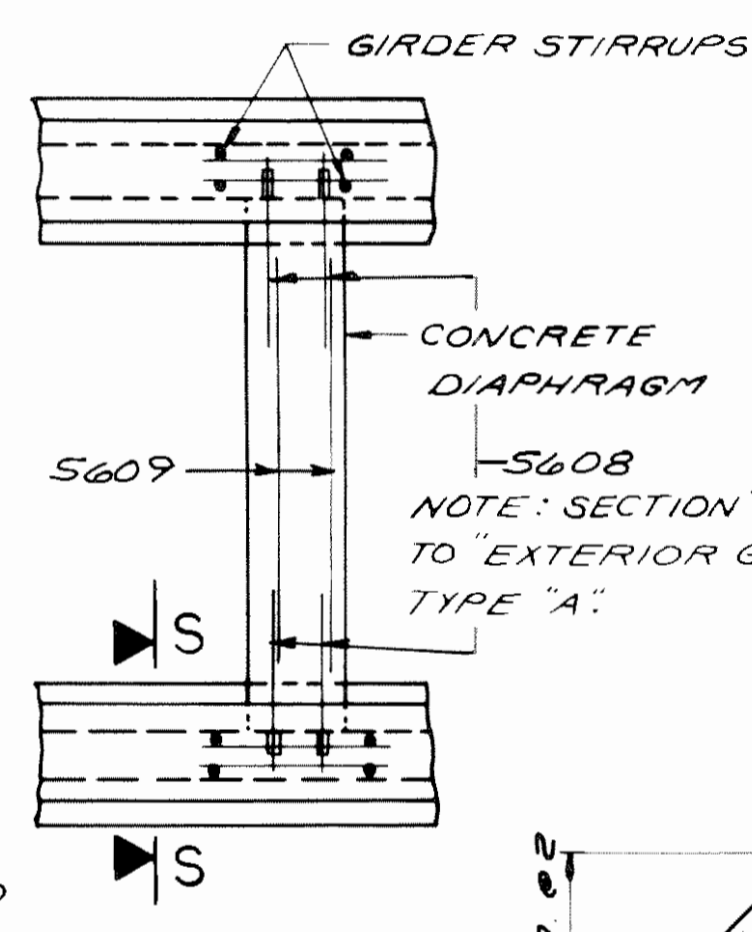
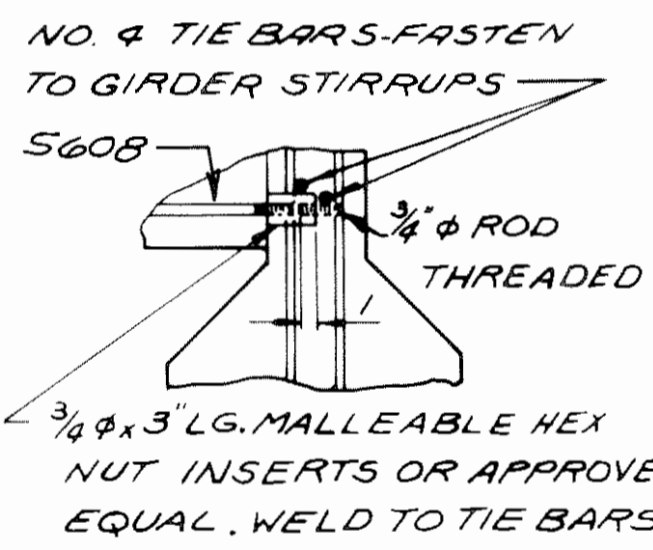
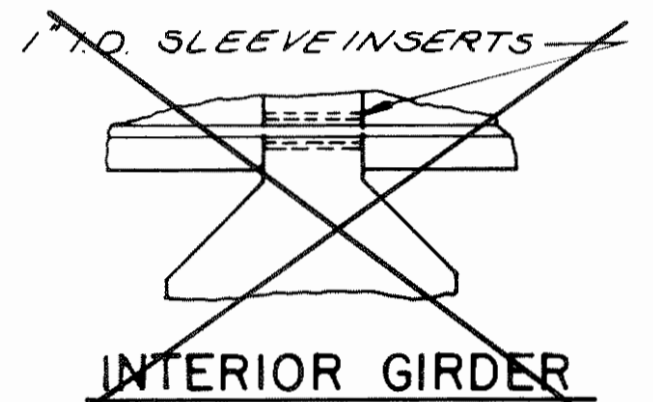
DRAPED STRAND PROFILE



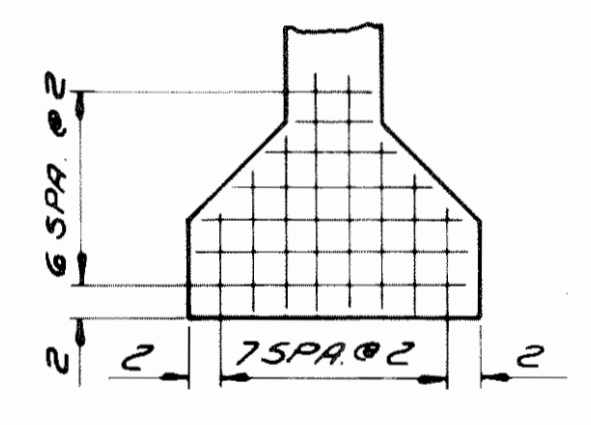
DEFLECTION DATA

* MINIMUM CYLINDER STRENGTH OF CONCRETE @ TIME OF TRANSFER OF PRESTRESS FORCE.
 ** TOTAL INITIAL PRESTRESS FORCE IN KIPS.

GIRDER DATA		USE DIAPHR. INSERTS DETAIL TYPE "B" USE NO. 6 "G" BARS, WITH A 3'-7" LG. LAP												
SPAN	GIRDER LENGTH "L"	DEFLECTION DATA (IN.)			TYPE OF STRANDS	DRAPED STRANDS					UNDRAPED STRANDS			
		"D"	"E"	"F"		TOTAL NO. OF STRANDS	f'ci (PSI)	"P"	"A"	"B"	"C"	TOTAL f'ci (PSI)	"P"	
1	56'-9"	1 3/8"	3/4"	7/8"	STRESS RELIEVED	22	5200	637	23	8 3/4"	11 3/4"	4		
						22	5200	637	23	8 3/4"	11 3/4"	4		
						22	5200	637	23	8 3/4"	11 3/4"	4		
						22	5200	637	23	8 3/4"	11 3/4"	4		

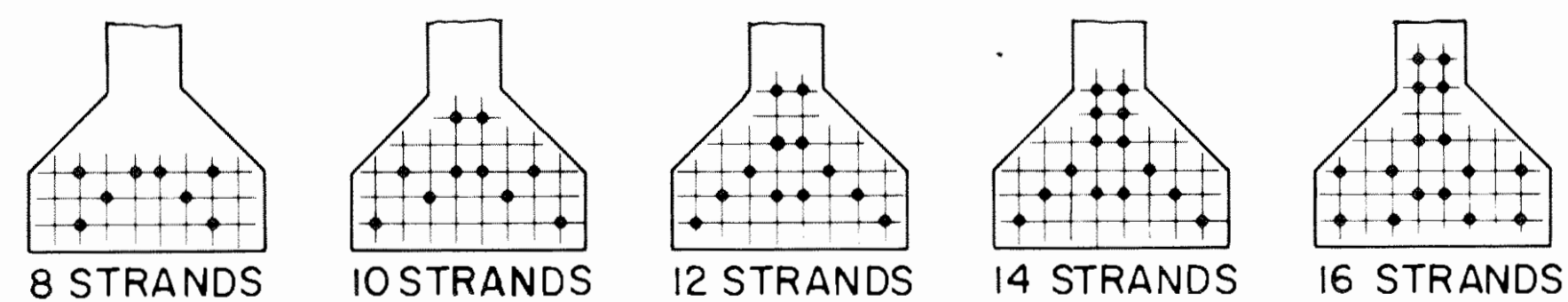


PLAN TYPE "B"

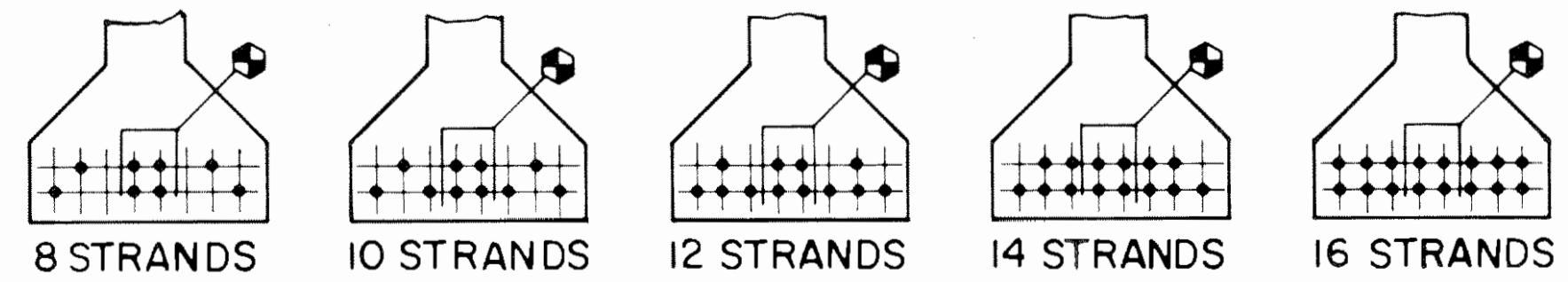


TYP. STRAND GRID

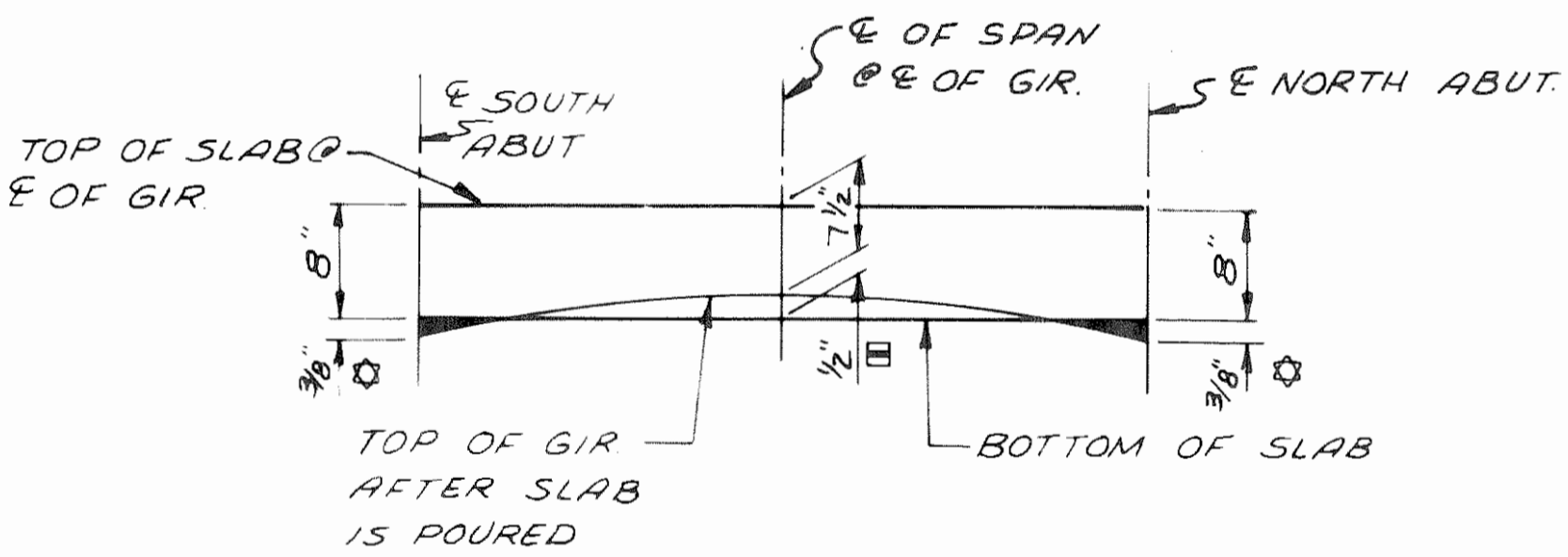
DIAPHRAGM INSERT DETAILS



UNDRAINED PATTERN

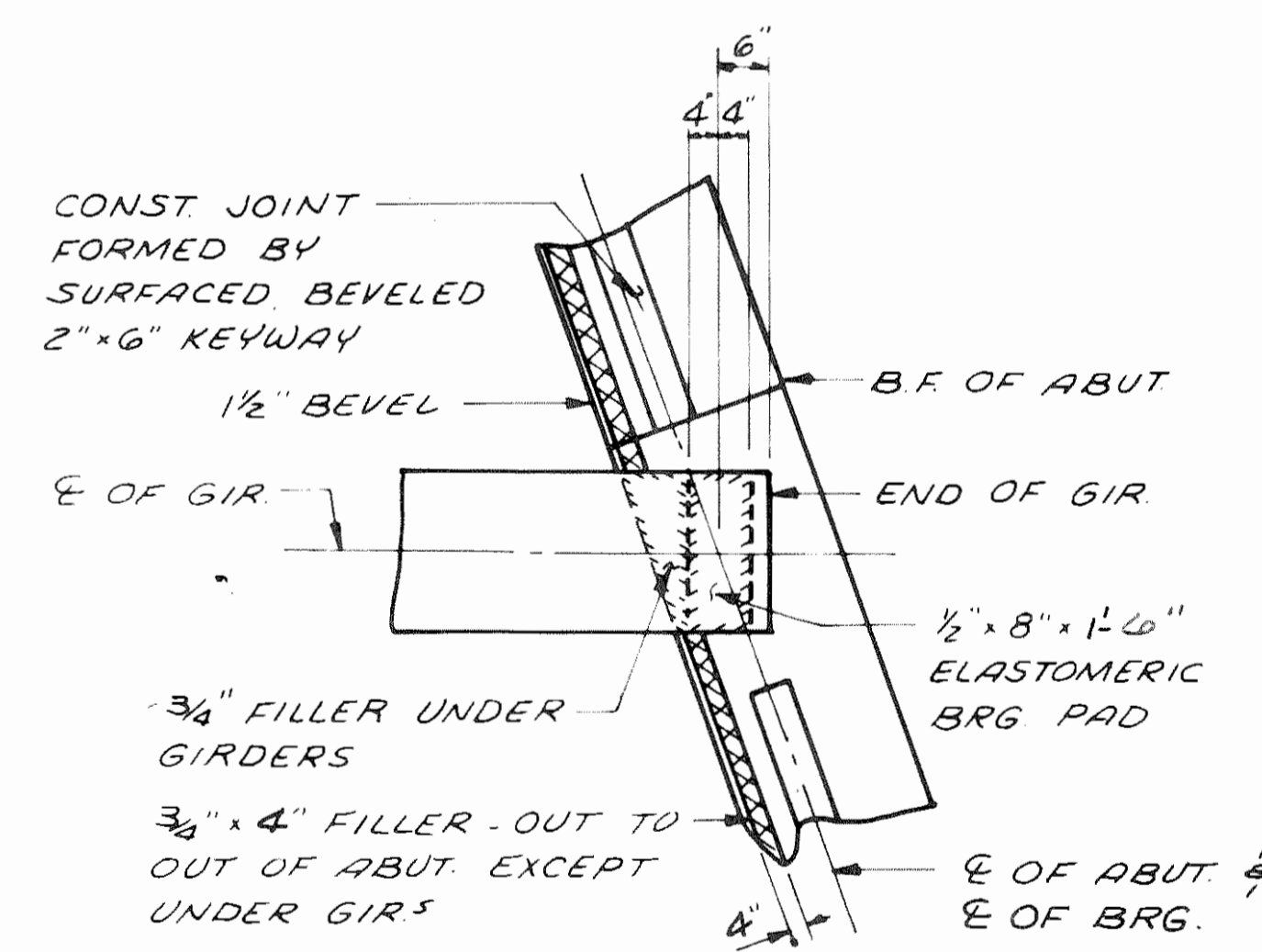


DRAPED PATTERN



SLAB FORMING DIAGRAM

TO COMPENSATE FOR VARIATIONS IN PRESTRESS CAMBER AND OTHER MINOR CONSTRUCTION DISCREPANCIES THE EMBEDMENT OF THE GIRDER INTO THE SLAB MAY BE VARIED WITH A MAXIMUM OF 1/2" ALLOWABLE EMBEDMENT AND THE SLAB HELD TO PLAN THICKNESS.
 IF VARIATIONS IN PRESTRESS CAMBER AND OTHER CONSTRUCTION DISCREPANCIES ARE OF SUCH A MAGNITUDE THAT THE MAXIMUM ALLOWABLE EMBEDMENT AS NOTED ABOVE WILL BE EXCEEDED THESE DIMS SHALL BE REVISED. THE 1/2" EMBEDMENT AND THE PLAN SLAB THICKNESS SHALL BE HELD WHILE THE GRADE LINE WILL BE REVISED.



ELASTOMERIC BEARING PAD DETAILS

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-35-84			
Const. Spec.	1981	Drawn By	NJA
		Plans Checked	RIS
36" PRESTRESSED GIRDER DETAILS			SHEET 9 OF 11
			X67860

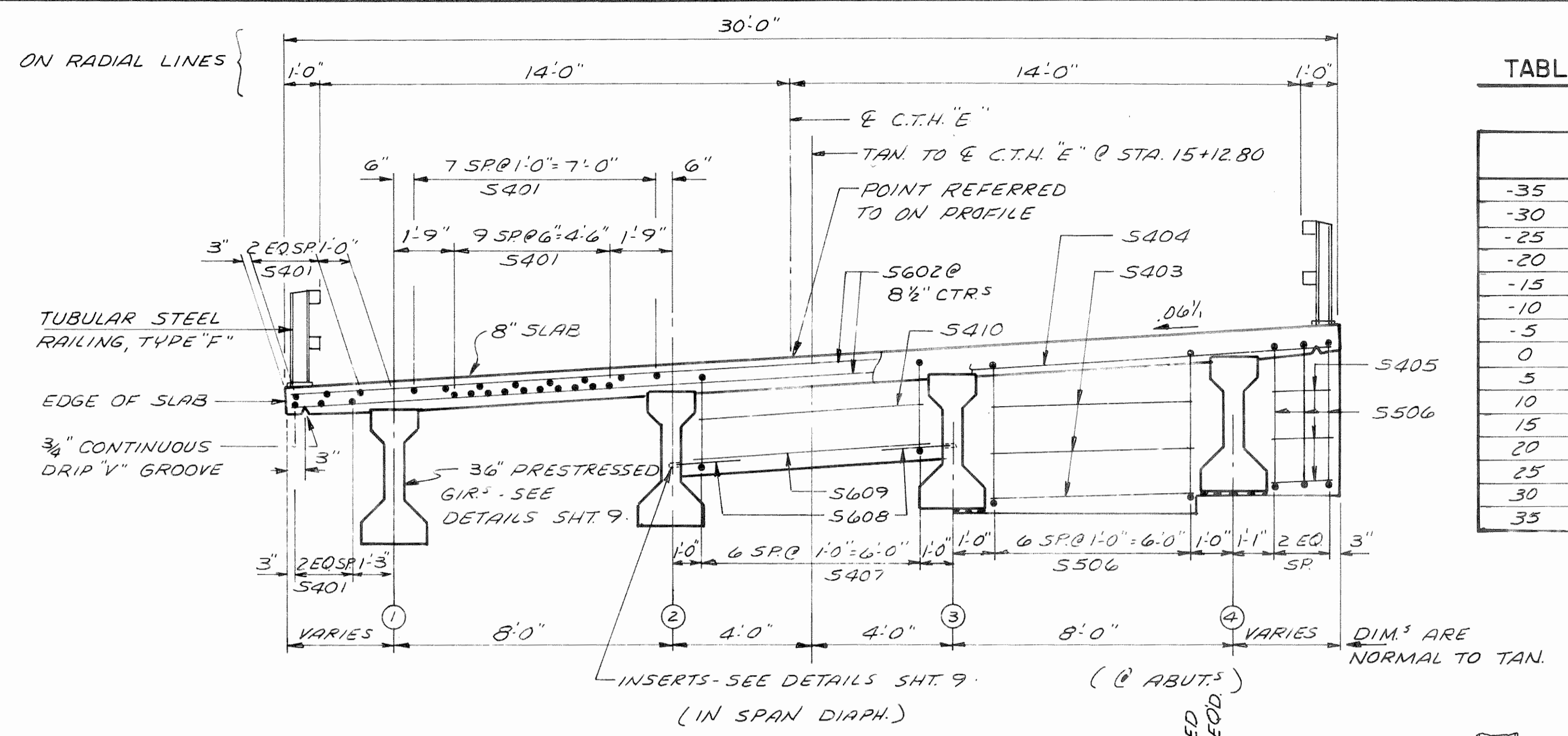
TABLE OF TANGENT OFFSETS
(IN FEET)

	DIM.		
	A	B	C
-35	0.64	14.368	15.652
-30	0.47	14.536	15.479
-25	0.33	14.678	15.333
-20	0.21	14.794	15.213
-15	0.12	14.884	15.120
-10	0.05	14.948	15.053
-5	0.01	14.987	15.013
0	0	15.00	15.00
5	0.01	14.987	15.013
10	0.05	14.948	15.053
15	0.12	14.884	15.120
20	0.21	14.794	15.213
25	0.33	14.678	15.333
30	0.47	14.536	15.479
35	0.64	14.368	15.652

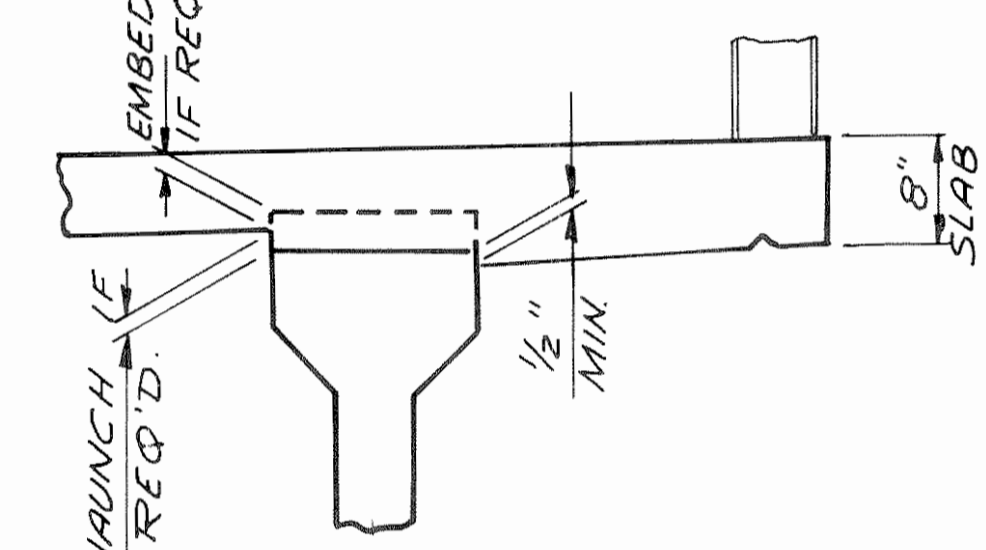
BILL OF BARS

THE FIRST DIGIT OF THE BAR MARK SIGNIFIES THE BAR SIZE.
DIM'S IN BENDING DETAILS ARE OUT TO OUT OF BAR.

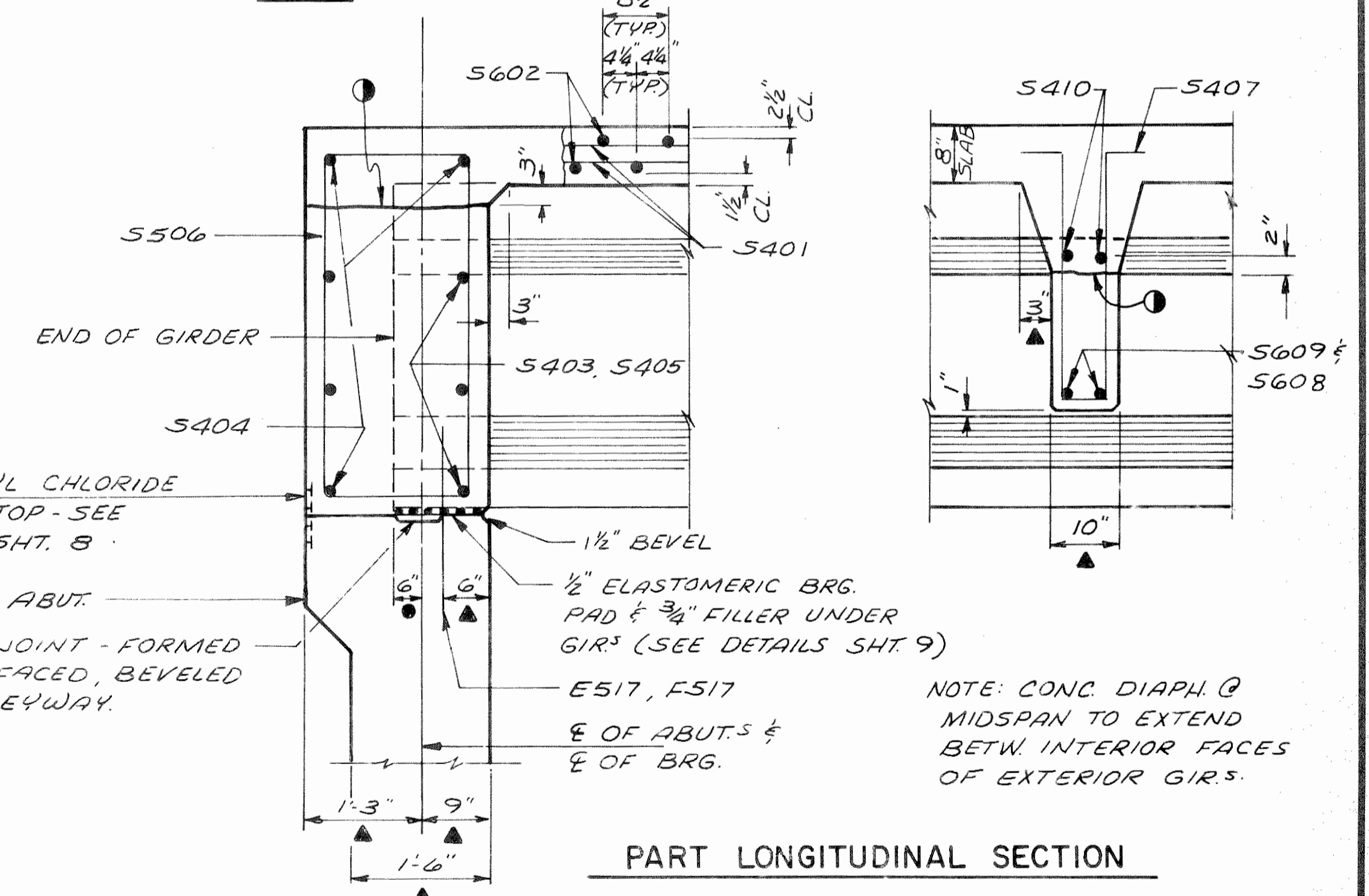
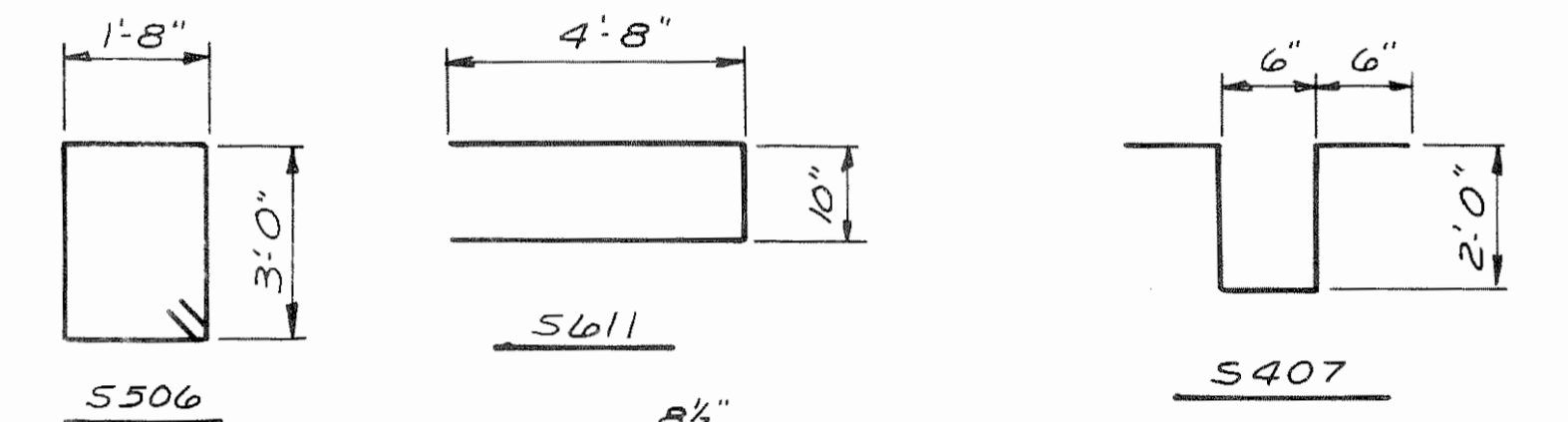
MARK	NO. REQ'D	LENGTH	B _{ENT}	LOCATION
S401	132	30-0		SLAB LONGIT.
S602	163	31-7		" TRANS.
S403	18	6-6		DIAPH. @ ABUT'S HORIZ.
S404	10	31-7		" @ " "
S405	12	2-0		" @ " "
S506	54	10-0	*	" @ " VERT.
S407	21	5-9	*	" IN SPAN "
S608	12	3-0		" " " HORIZ.
S609	6	7-4		" " " "
S410	6	6-8		" " " "
S611	16	10-0	*	SLAB @ RAIL POSTS
S612	32	4-0		" @ " "



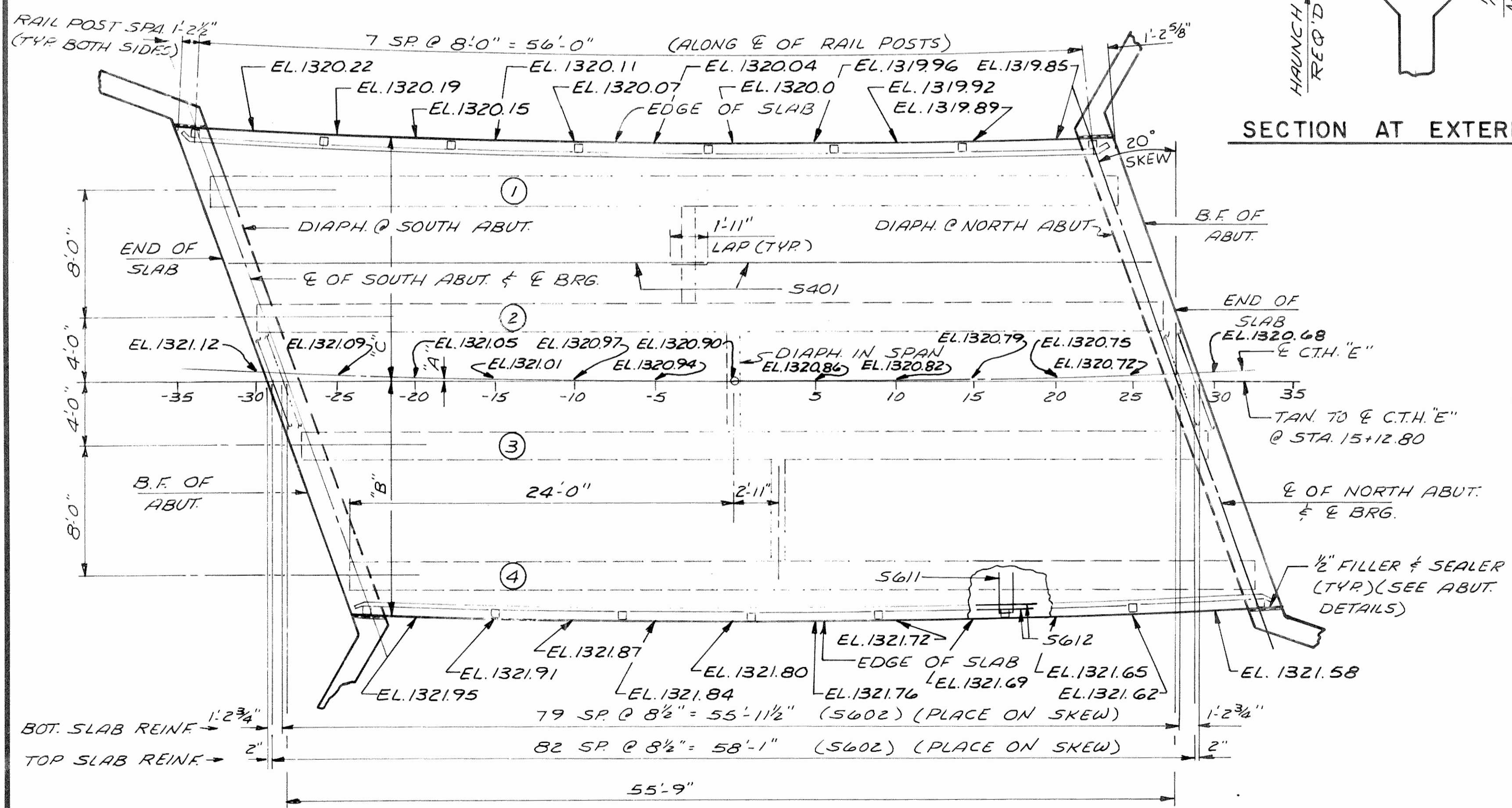
CROSS SECTION THRU ROADWAY
(LOOKING NORTH)



SECTION AT EXTERIOR GIRDER



PART LONGITUDINAL SECTION



PLAN

POLYVINYL CHLORIDE WATERSTOP - SEE DETAIL SHT. 8.
B.F. OF ABUT.
CONST. JOINT - FORMED BY SURFACED, BEVELED 2" x 6" KEYWAY.

NOTE: CONC. DIAPH. @ ABUT'S TO EXTEND OUT TO OUT OF SLAB.

- ▲ DIM'S ARE NORMAL TO & SUBSTRUCTURE UNIT.
- DIM'S ARE PARALLEL TO & OF GIRDERS.
- OPTIONAL CONSTRUCTION JOINT. STRIKE OFF & LEAVE ROUGH.

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-35-84			
Const. Spec.	1981	Drawn By	NJA
		Plans Checked	RIS
SUPERSTRUCTURE			SHEET 10 OF 11
			X67861

SCHEDULE OF LANDS AND INTERESTS REQUIRED

PARCEL NUMBER	OWNER	INTEREST REQUIRED	L.H.E. ACRES	ACRES REQUIRED			TOTAL REMAINING ACRES	OPERATIONS PROJECT NO.
				NEW R/W REQUIRED	EXISTING R/W	TOTAL R/W REQUIRED		
1	EMIL & ESTHER OESTREICH	FEE	0.02	0.30	0.30	0.60		9411-01-00
2	LLOYD & VIRGINIA OESTREICH	"	—	0.01	0.10	0.11		"
3	RALPH E. SCHULTZ	"	—	0.01	—	0.01		"
4	REINHARDT LANDS, INC.	"	0.28	0.28	0.24	0.52		"
5	DONALD L. & CAROL A. KAATZ	"	—	0.08	0.14	0.22		"
6	JOSEPH & CAROL ALERY	"	0.15	0.34	0.84	1.18		"
7	COUNTRY SQUIRE LANDS, INC.	"	0.64	—	—	—		"
8	GENERAL TELEPHONE CO.	RELEASE OF RIGHTS	—	—	—	—		"

ALL R/W IS REFERENCED TO THE CONSTRUCTION & BEARINGS SHOWN ON THE PLAT ARE MAGNETIC BEARINGS TO THE NEAREST MINUTE. ALL COORDINATES SHOWN ON THIS PLAT ARE REFERENCED TO THE WISCONSIN COORDINATE SYSTEM, CENTRAL ZONE, AND SCALED FROM THE ALEXANDER LAKE QUADRANGLE FOR IDENTIFICATION PURPOSES ONLY.

REVISION DATE	R/W PROJECT NUMBER	SHEET NUMBER
	9411-01-00	
FEDERAL PROJECT NUMBER		
PLAT OF RIGHT OF WAY REQUIRED FOR C.T.H. "E" LINCOLN COUNTY		
SCALE	DATE	
1" = 200 FT.	8-11-63	
CONSTRUCTION PROJECT NUMBER		
9411-01-71		
APPROVED FOR LINCOLN COUNTY BY		
DATE	COUNTY	HIGHWAY COMMISSIONER

LEGEND

- ⊙ TELEPHONE POLE
- ⊠ UNDERGROUND TELEPHONE RISER
- ⊡ POWER POLE
- UNDERGROUND TELEPHONE CABLE
- IRON STAKE
- LATHE

STANDARD ABBREVIATIONS

- PL. PROPERTY LINE
- T TRANSIT LINE
- ⊕ CENTERLINE
- R/W RIGHT OF WAY
- L.H.E. LIMITED HIGHWAY EASEMENT
- REQ'D REQUIRED

CURVE DATA

CONSTRUCTION &	SURVEY I
PI. = 14+79.70	PI. = 14+79.70
Δ = 39°-18'-00"	Δ = 39°-18'-00"
D = 6°-00'	D = 7°-00'
R = 954.93'	R = 818.51'
T = 340.97'	T = 292.26'
L = 655.00'	L = 561.43'
E = 59.05'	E = 50.61'

