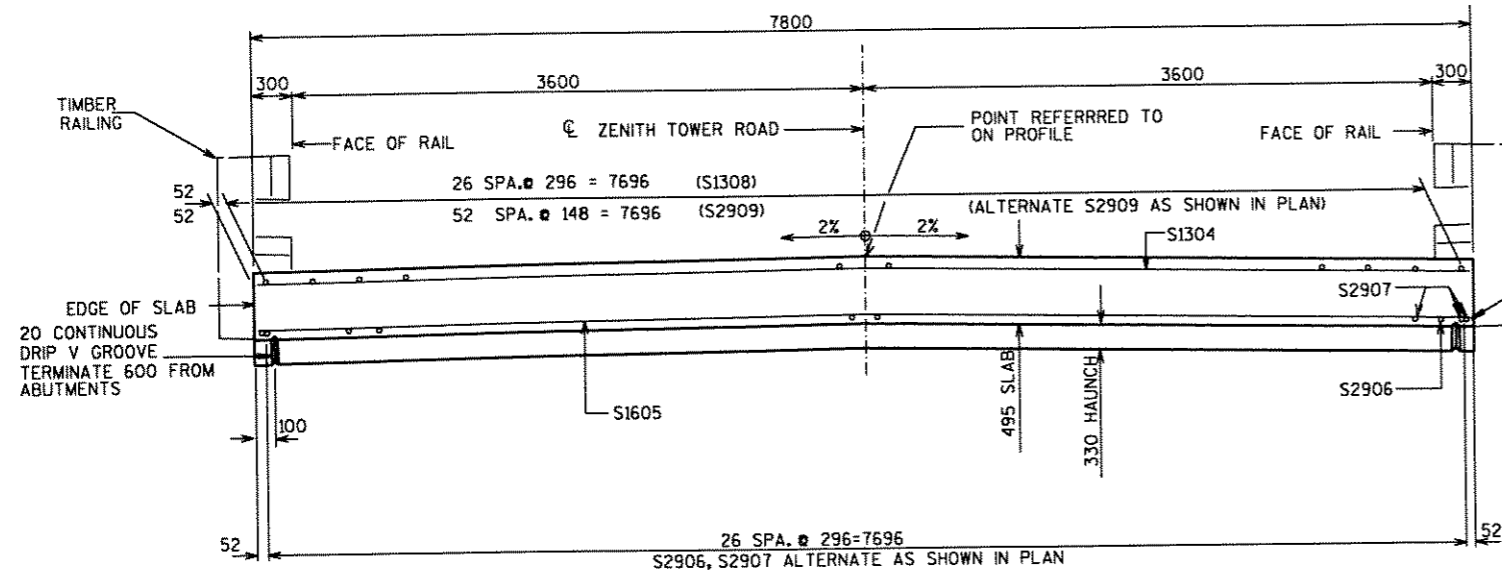


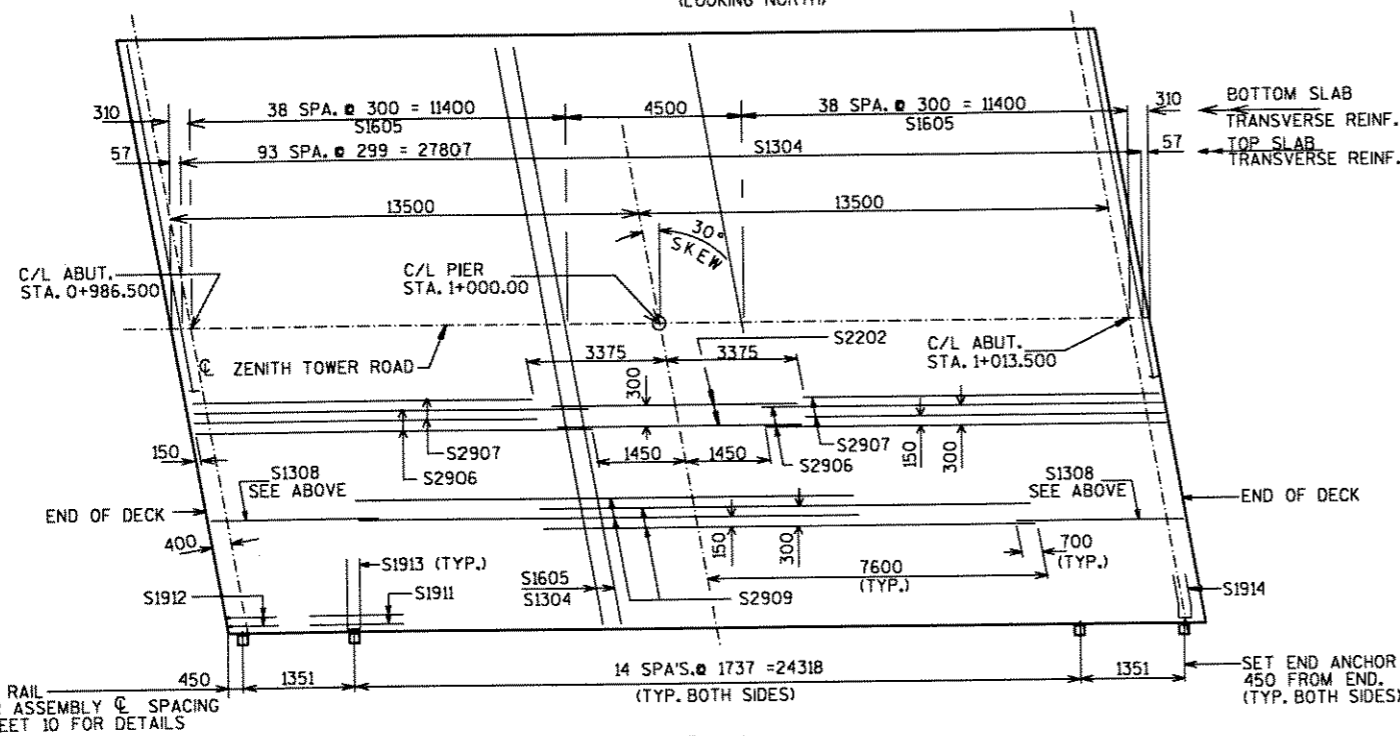
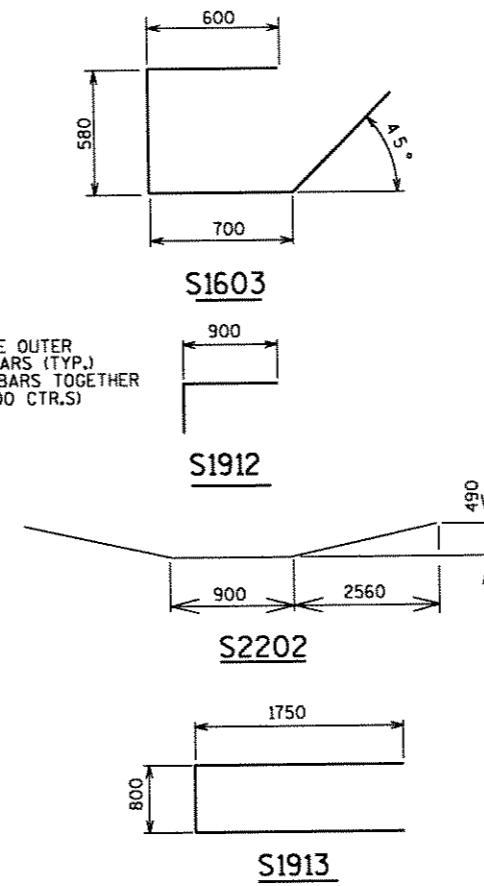
**BILL OF BARS**

THE FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.  
DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.  
EPOXY COAT ALL TOP BARS.

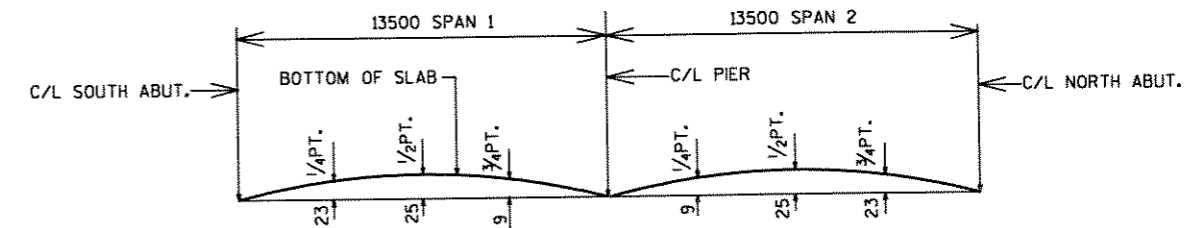
BAR MARK	NO. REQ'D.	LENGTH	BENT	COAT	LOCATION
S2202	27	6100	X		HAUNCH AT PIER
S1603	62	2350	X	ALL	AT END OF DECK
S1304	98	8890		ALL	SLAB, TOP, TRANSVERSE
S1605	88	8890			SLAB, BOTTOM, TRANSVERSE
S2906	54	12360			SLAB, BOTTOM, LONGIT.
S2907	54	10435			SLAB, BOTTOM, LONGIT.
S1308	58	7000		ALL	SLAB, TOP, LONGIT.
S2909	53	12330		ALL	SLAB, TOP, LONGIT.
S1911	60	1200		ALL	AT INTERIOR RAIL POSTS
S1912	8	1200	X	ALL	AT END RAIL POSTS
S1913	30	4200	X	ALL	AT INTERIOR RAIL POSTS
S1914	4	4200	X	ALL	AT END RAIL POSTS



**CROSS SECTION THRU RDWY.**  
(LOOKING NORTH)

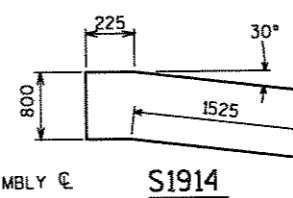


**PLAN**

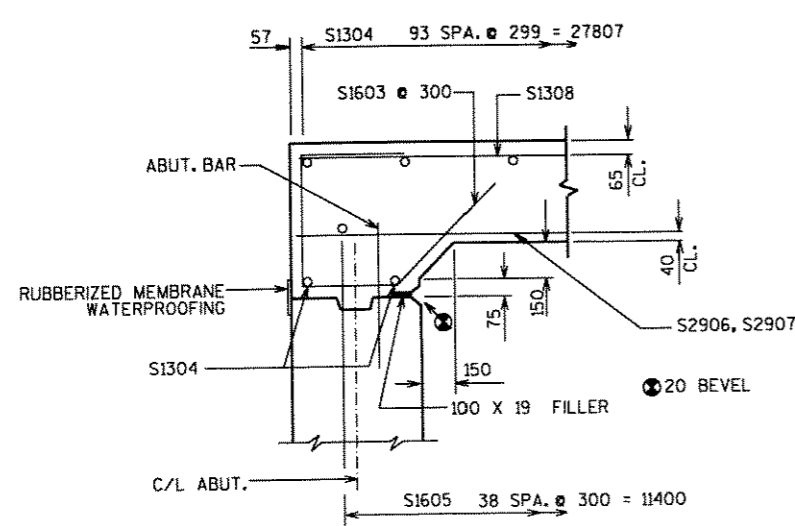


**CAMBER DIAGRAM**

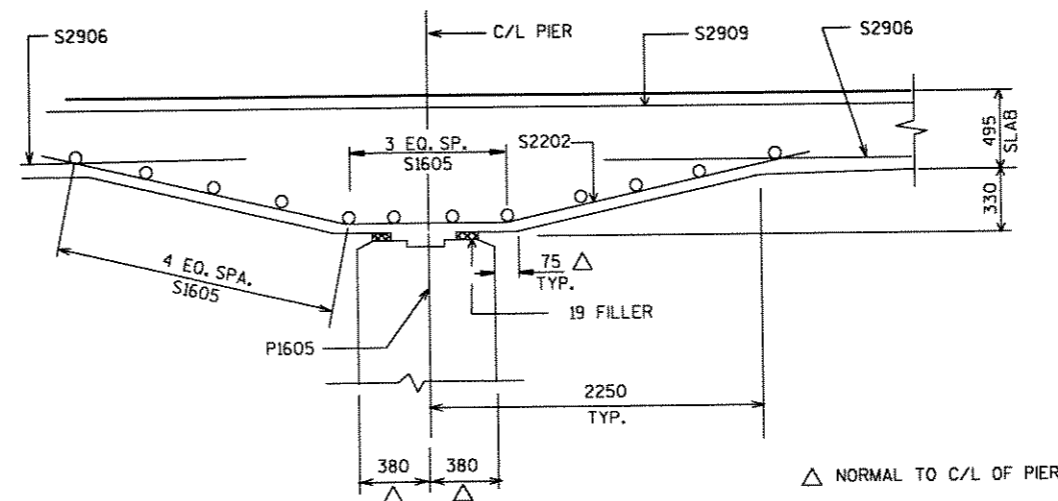
CAMBER SPAN AS SHOWN TO PROVIDE FOR DEADLOAD DEFLECTION & FUTURE PLASTIC FLOW. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT.



**S1914**



**DETAILS AT ABUTMENTS**



**DETAILS HAUNCH AT PIER**

ALTERNATE TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 900mm CENTERS. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 1200mm CENTERS.

TRANSVERSE BARS SHALL BE PLACED PARALLEL TO THE C/L OF SUBSTRUCTURE UNITS.

THE SLAB THICKNESS DIMENSION IS MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
<b>STRUCTURE B-35-137</b>			
CONST. SPEC.	1996	DRAWN BY NJA	PLANS CKD. CJB
<b>SUPERSTRUCTURE</b>			SHEET 9 OF 10