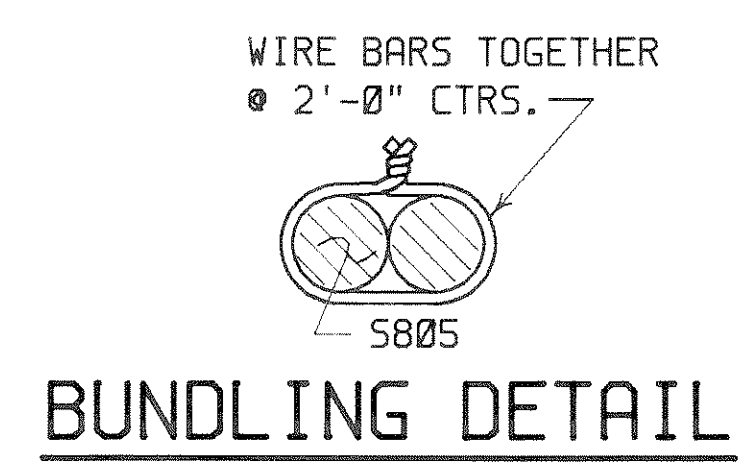


ALTERNATE TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS @ APPROX. 3'-0" CTRS., BTM. LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS @ APPROX. 4'-0" CTRS.



### BILL OF BARS

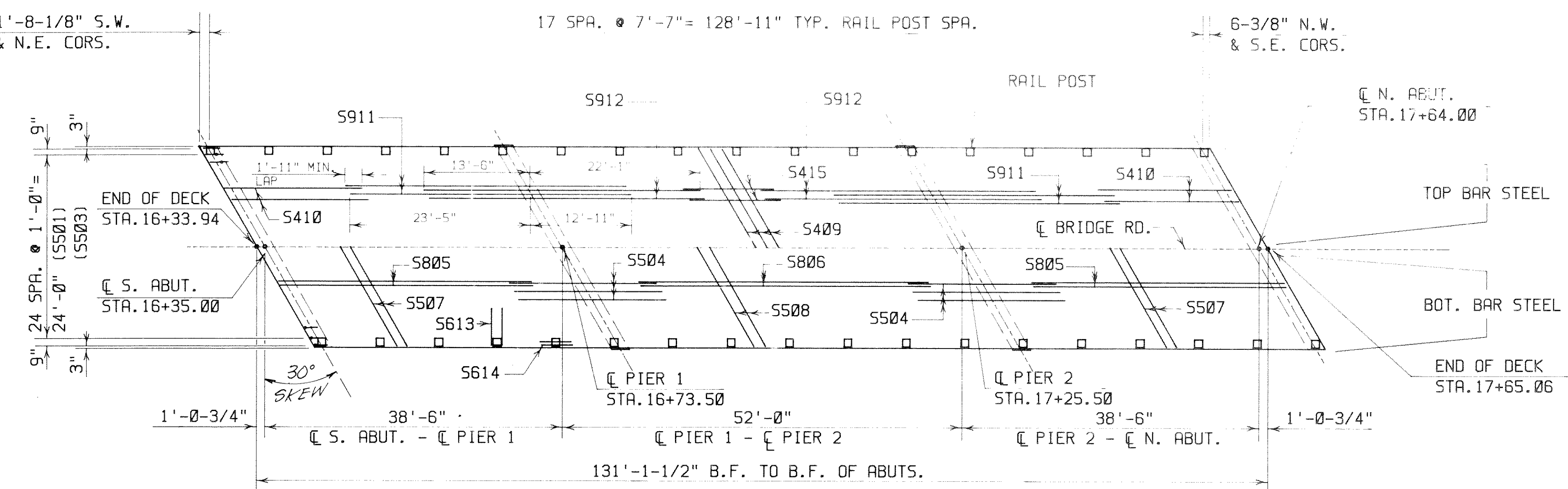
MARK	NO.	LENGTH	BENT	CUT	LOCATION
S501	54	3'-6"	X		HAUNCH @ ABUTS. STIRRUPS
S402	16	29'-7"			HAUNCH @ ABUTS. & PIERS TRANS.
S503	54	3'-1"	X		HAUNCH @ ABUTS. STIRRUPS
S504	54	19'-2"	X		HAUNCH @ PIERS LONGIT.
S805	104	32'-10"			SLAB BTM. SPANS 1&3 LONGIT.
S806	52	39'-0"			SLAB BTM. SPAN 2 LONGIT.
S507	50	29'-7"			SLAB BTM. SPANS 1&3 TRANS.
S508	25	29'-7"			SLAB BTM. SPAN 2 TRANS.
S409	87	29'-7"			SLAB TOP TRANS.
S410	36	17'-11"			SLAB TOP SPANS 1&3 LONGIT.
S911	52	36'-4"			SLAB @ PIERS TOP LONGIT.
S912	52	35'-7"			SLAB @ PIERS TOP LONGIT.
S613	36	10'-0"	X		SLAB 1 @ E.A. RAIL POST
S614	72	4'-0"			SLAB 2 @ E.A. RAIL POST
S415	18	11'-9"			SLAB TOP SPAN 2 LONGIT.

19,095# (UNCOATED)  
15,500# (COATED)

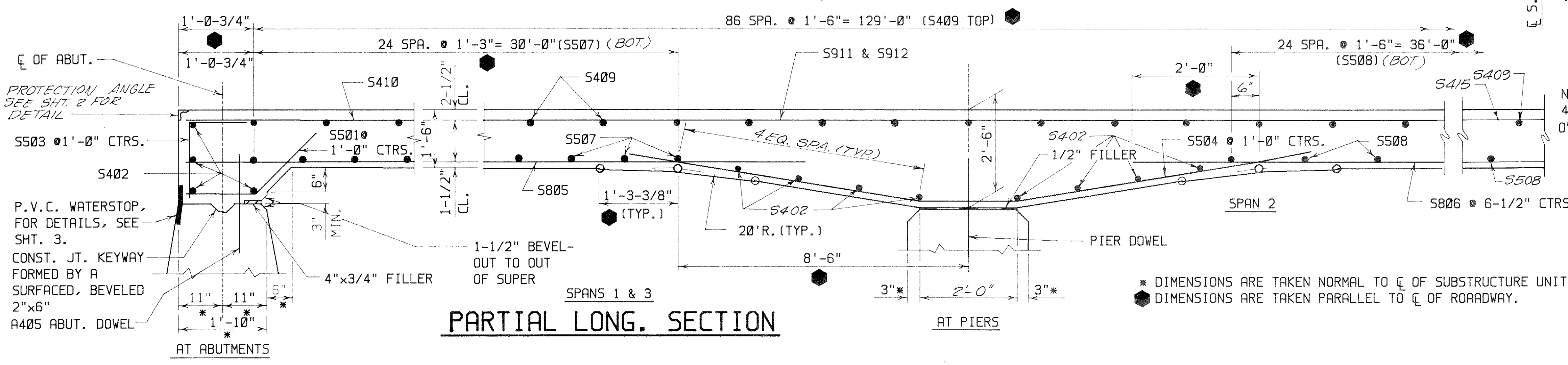
THE FIRST DIGIT OF A THREE DIGIT BAR MARK SIGNIFIES BAR SIZE. DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BARS.  
\* THESE BARS TO BE COATED

ALL SLAB DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT ANY CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

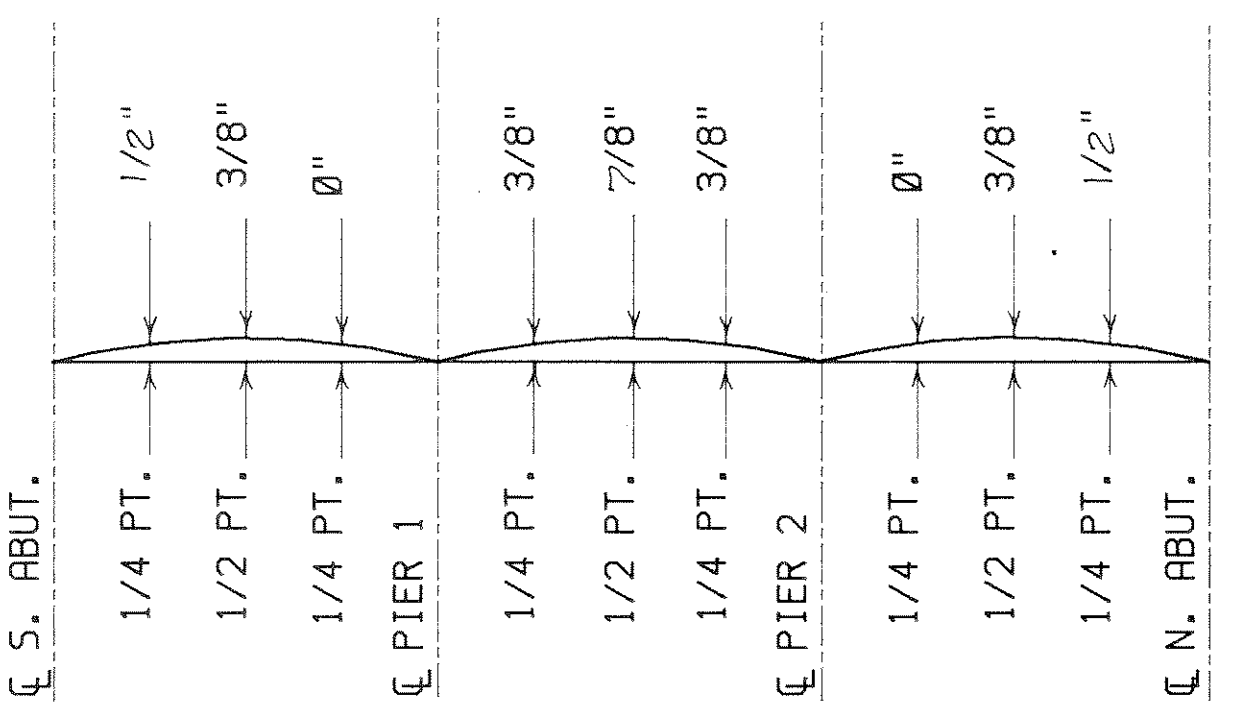
PROVIDE CAMBER AS SHOWN BELOW TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE PLASTIC FLOW. THIS DOES NOT INCLUDE AN ALLOWANCE FOR FORM SETTLEMENT. DEADLOAD DEFLECTION ONLY EQUALS APPROXIMATELY 1/4 OF CAMBER VALUES SHOWN.



### PLAN



### PARTIAL LONG. SECTION



### CAMBER DIAGRAM

NOTE: THE CONC. IN ANY SPAN SHALL BE PLACED WITHIN 4 HOURS OF THE TIME THAT CONC. WAS PLACED OVER THE ADJACENT PIER.

\* DIMENSIONS ARE TAKEN NORMAL TO C OF SUBSTRUCTURE UNITS.  
● DIMENSIONS ARE TAKEN PARALLEL TO C OF ROADWAY.

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
<b>STRUCTURE B-35-91</b>			
Const. Spec. WIS. '81	Drawn By TL	Plans Checked L.M.B	
<b>SUPERSTRUCTURE</b>			SHEET 5 OF 6 X 77183