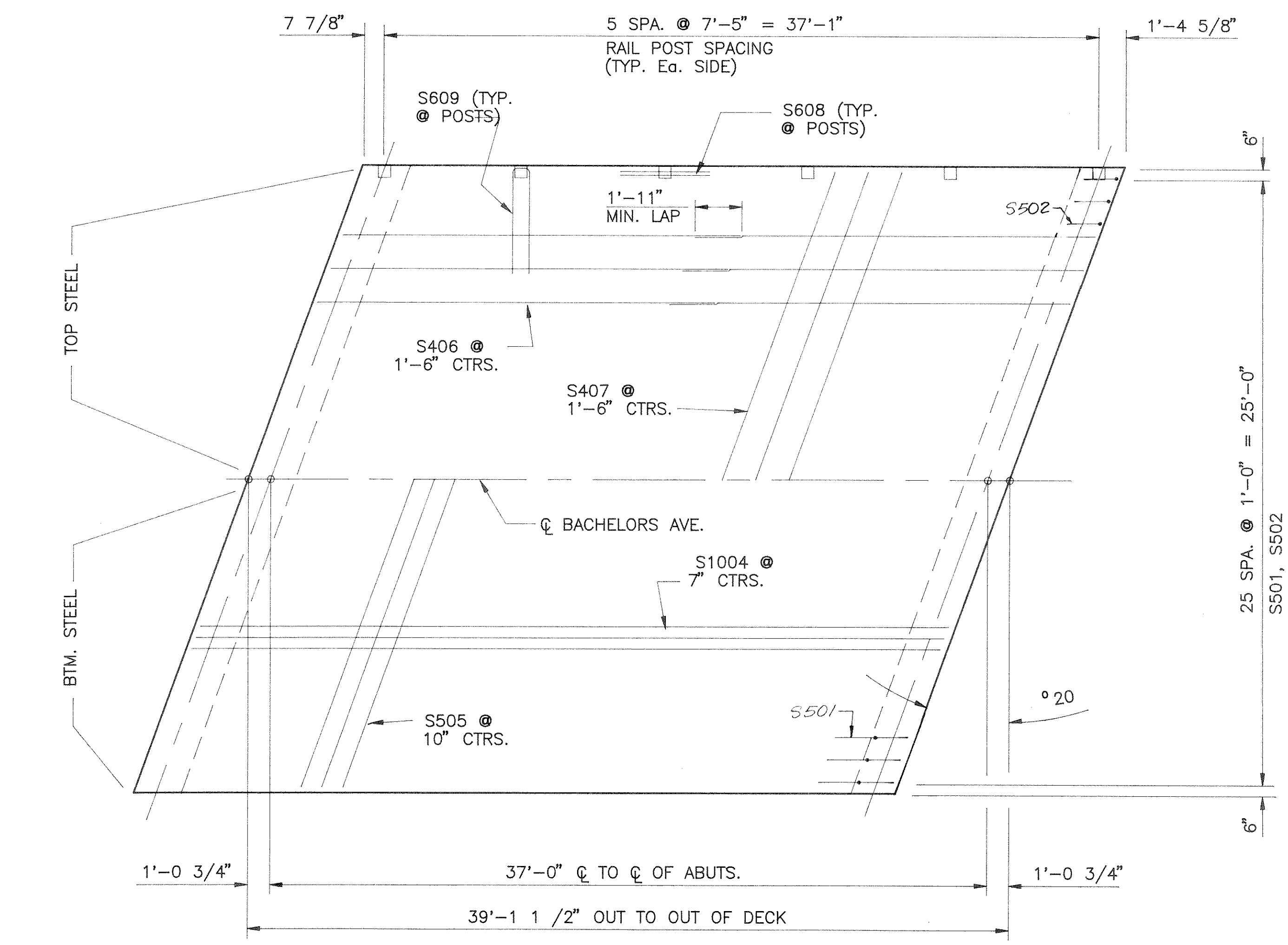
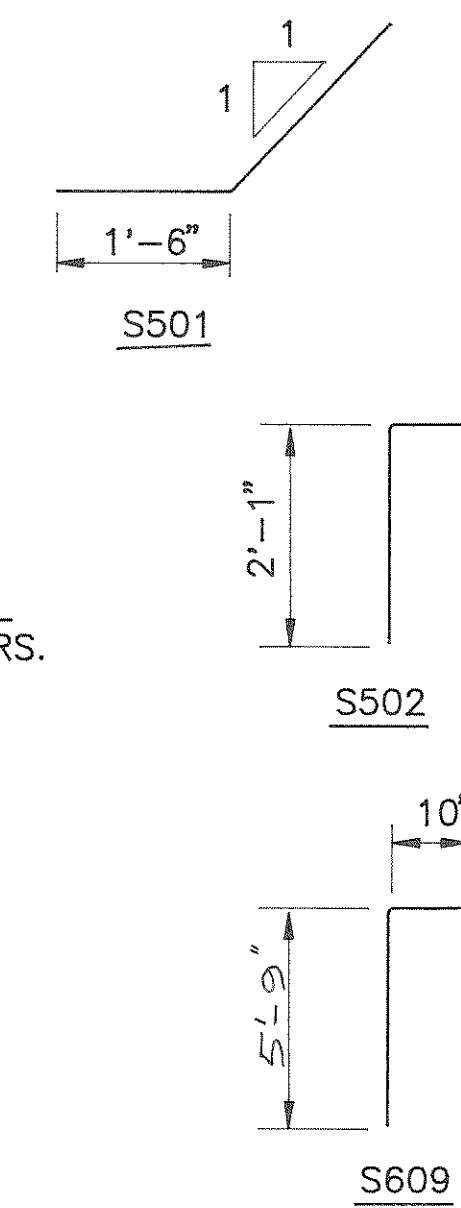


CROSS SECTION THRU ROADWAY



PLAN

NOTE: ALTERNATE TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CTRS. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CTRS.

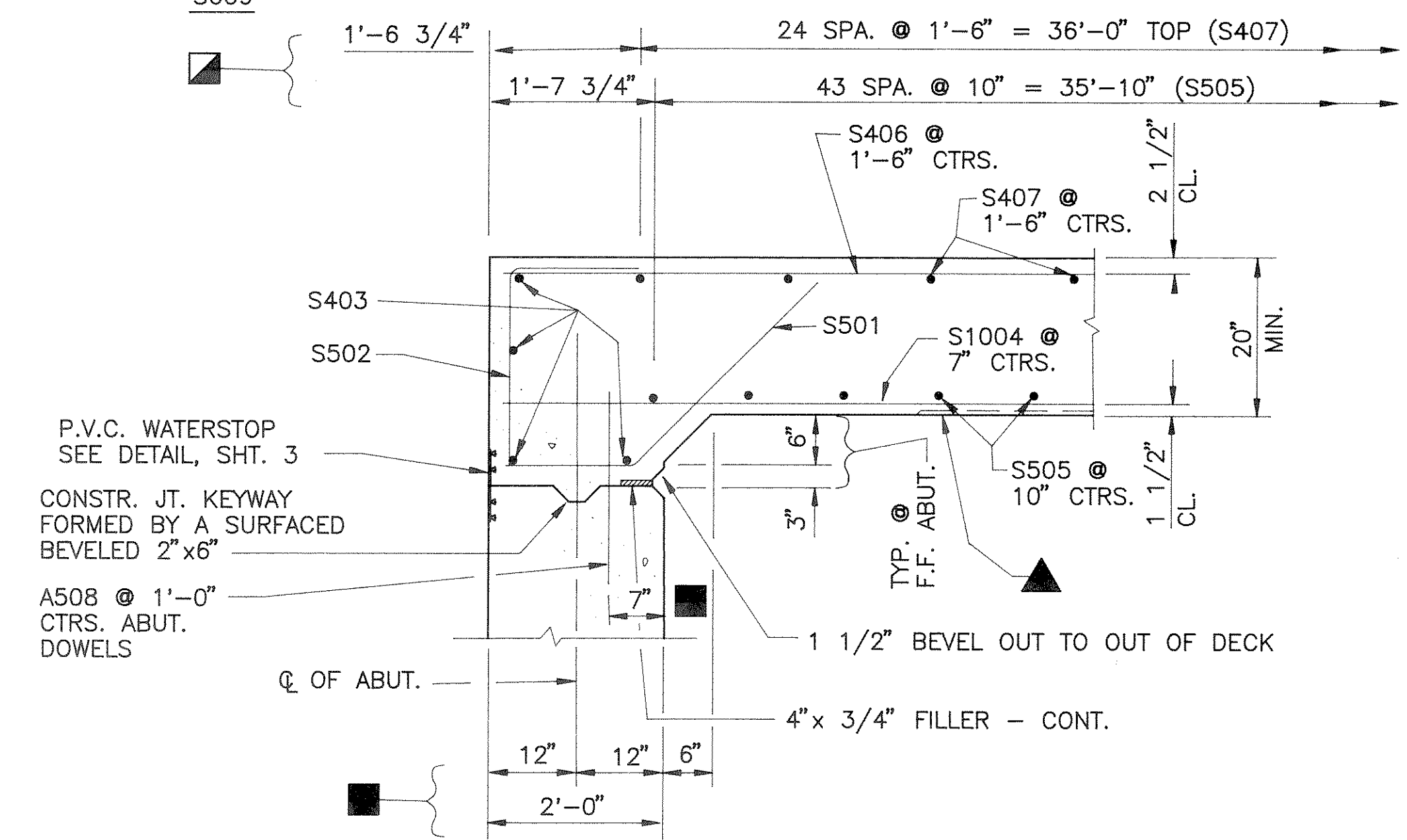


BILL OF BARS

11,065 #

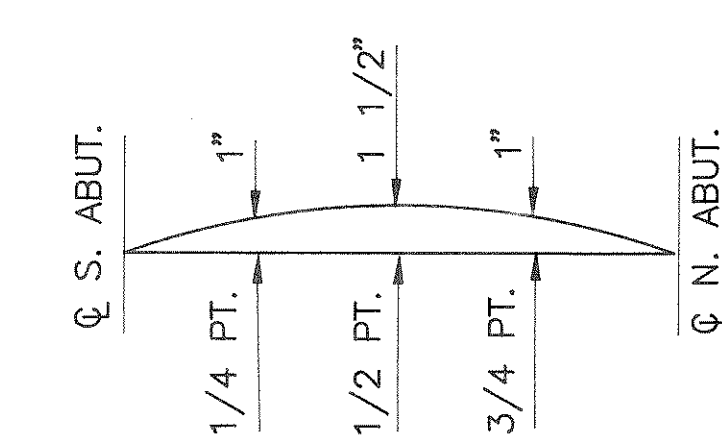
MARK	NO.	LENGTH	COAT	BENT	LOCATION
S501	52	4'-0"	X	X	HAUNCH @ ABUT.- VERT. STIRRUP
S502	52	3'-3"	X	X	HAUNCH @ ABUT.- VERT. STIRRUP
S403	8	27'-3"	X		HAUNCH @ ABUT. HORIZ.
S1004	48	38'-9"			SLAB - LONGIT. - BTM.
S505	44	27'-3"			SLAB - TRANSV. - BTM.
S406	36	20'-4"	X		SLAB - LONGIT. - TOP
S407	25	27'-3"	X		SLAB - TRANSV. - TOP
S608	24	4'-0"	X		SLAB @ RAIL POSTS - 2 Ea. POSTS
S609	12	12'-0"	X	X	SLAB @ RAIL POSTS

THE FIRST DIGIT, OR THE FIRST TWO DIGITS OF A BAR MARK SIGNIFIES THE BAR SIZE. DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BARS.



PARTIAL LONGITUDINAL SECTION

- DIMENSION IS GIVEN NORMAL TO ABUTMENT.
- ▣ DIMENSION IS GIVEN NORMAL TO CL OF ROADWAY.
- ▲ 3/4" CONTINUOUS "V" DRIP GROOVE TERMINATE 2'-0" FROM Ea. ABUTMENT



CAMBER DIAGRAM

PROVIDE CAMBER AS SHOWN ABOVE TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE PLASTIC FLOW THIS DOES NOT INCLUDE AN ALLOWANCE FOR FORM SETTLEMENT. DEAD LOAD DEFLECTION ONLY EQUALS APPROXIMATELY 1/4 OF CAMBER VALUES SHOWN.

No.	Date.	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-35-106			
Const. Spec.	WIS.'89	Drawn By	T.L.
		Plans Checked	S.R.L.
SUPERSTRUCTURE			SHEET 5 OF 6
			X 82700