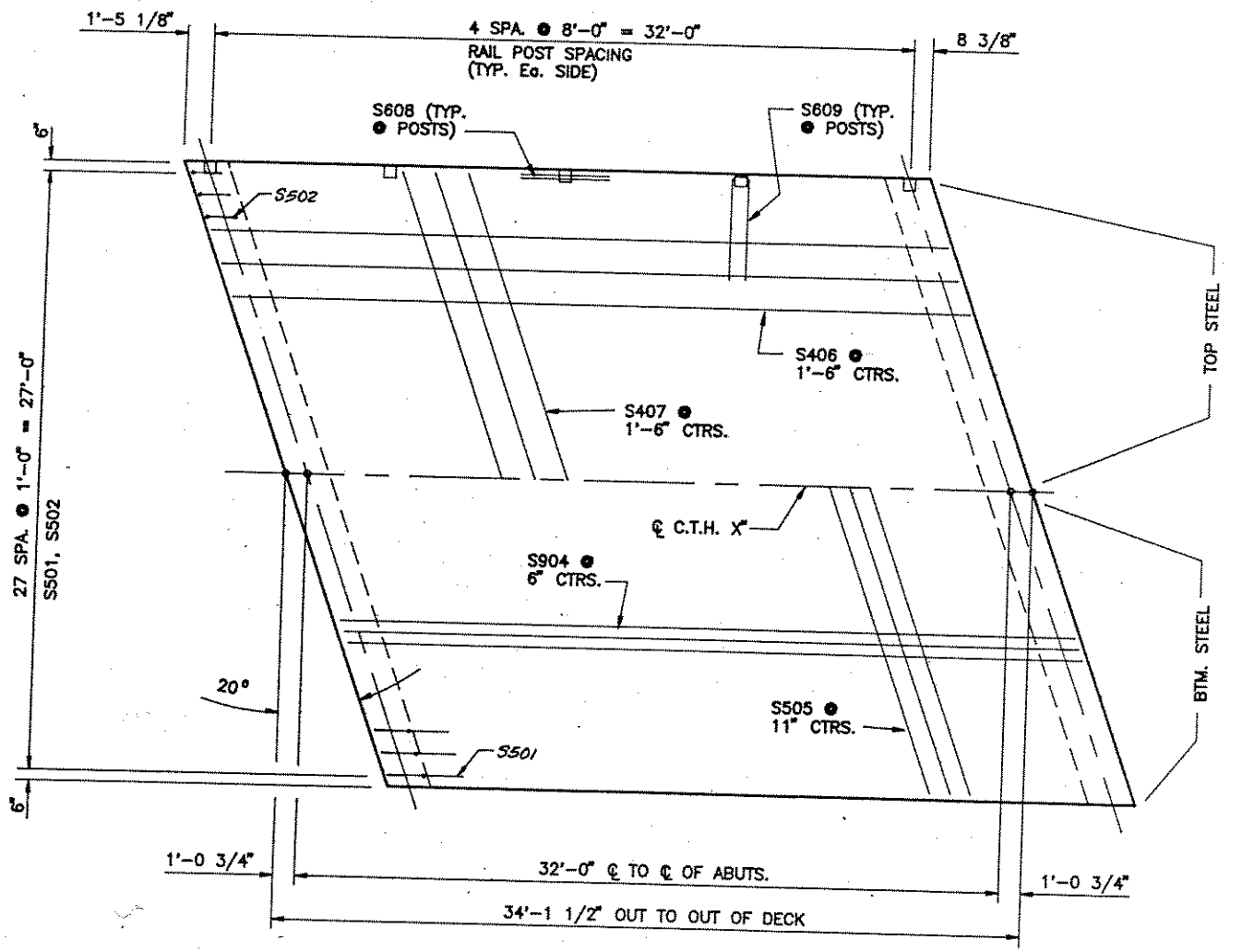
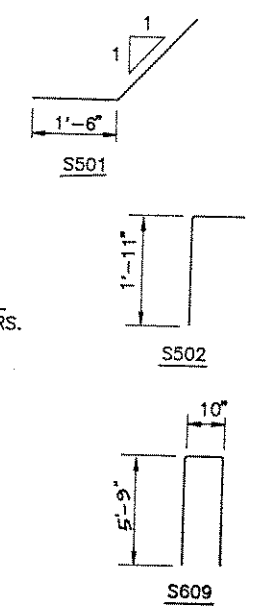


CROSS SECTION THRU ROADWAY

NOTE: ALTERNATE TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CTRS. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CTRS.



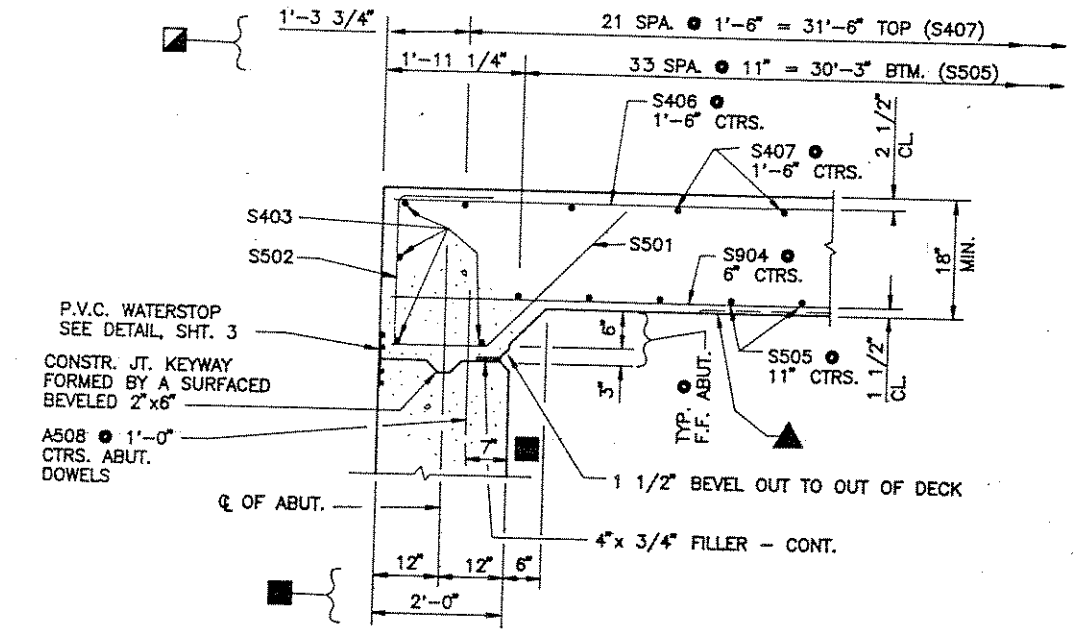
PLAN



BILL OF BARS

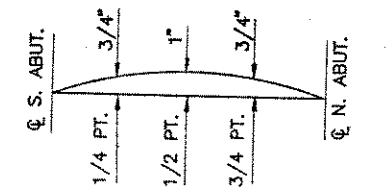
MARK	NO.	LENGTH	COAT	BENT	LOCATION
S501	56	3'-6"	X	X	HAUNCH @ ABUT. - VERT. STIRRUP
S502	56	3'-0"	X	X	HAUNCH @ ABUT. - VERT. STIRRUP
S403	8	29'-5"	X		HAUNCH @ ABUT. - HORIZ.
S904	60	33'-9"			SLAB - LONGIT. - BTM.
S505	34	29'-5"			SLAB - TRANSV. - BTM.
S406	19	33'-9"	X		SLAB - LONGIT. - TOP
S407	22	29'-5"	X		SLAB - TRANSV. - TOP
S608	20	4'-0"	X		SLAB @ RAIL POSTS - 2 Ea. POSTS
S609	10	12'-0"	X	X	SLAB @ RAIL POSTS

THE FIRST DIGIT OF THE BAR MARK SIGNIFIES THE BAR SIZE. DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BARS.



PARTIAL LONGITUDINAL SECTION

- DIMENSION IS GIVEN NORMAL TO ABUTMENT.
- ▣ DIMENSION IS GIVEN NORMAL TO ∅ OF ROADWAY.
- ▲ 3/4" CONTINUOUS V Drip GROOVE TERMINATE 2'-0" FROM Ea. ABUTMENT



CAMBER DIAGRAM

PROVIDE CAMBER AS SHOWN ABOVE TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE PLASTIC FLOW THIS DOES NOT INCLUDE AN ALLOWANCE FOR FORM SETTLEMENT. DEAD LOAD DEFLECTION ONLY EQUALS APPROXIMATELY 1/4 OF CAMBER VALUES SHOWN.

No.	Date.	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-35-105			
Const. Spec.	WIS. '89	Drawn By	T.L.
Plans Checked	S.R.L.		
SUPERSTRUCTURE			SHEET 5 OF 6
			X 82686