

**TYPICAL ROADWAY CROSS SECTION**  
STA. 10+00 TO STA. 23+00

**NOTE:**  
ALL HORIZONTAL DIMENSIONS AND VERTICAL DEPTHS REMAIN THE SAME THROUGH SUPER-ELEVATED SECTIONS.  
SLOPE VARIES FROM 4.77:1 ON THE HIGH SIDE TO 3.05:1 ON THE LOW SIDE.

**GENERAL NOTES**

FILL, AS SHOWN ON THE PLAN SHEETS, PERTAINS TO EMBANKMENT CONSTRUCTED FROM BORROW EXCAVATION AND/OR UNCLASSIFIED EXCAVATION. THE SHRINKAGE ALLOWANCE USED TO COMPUTE THE VOLUME OF MATERIAL NECESSARY TO COMPLETE THE FILL WAS 25% FOR UNCLASSIFIED EXCAVATION SOUTH OF STRUCTURE AND 20% FOR UNCLASSIFIED EXCAVATION NORTH OF STRUCTURE. EROSION BALES ARE TO BE DISTRIBUTED AS DETERMINED BY THE ENGINEER.

SEED MIXTURE NUMBER 1 SHALL BE USED.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCLUSIVE OF THE ROADBED, ARE TO BE FERTILIZED AND SEEDED AS DIRECTED BY THE ENGINEER.

BEARINGS SHOWN ON THE PLANS ARE MAGNETIC BEARINGS TO THE NEAREST MINUTE.

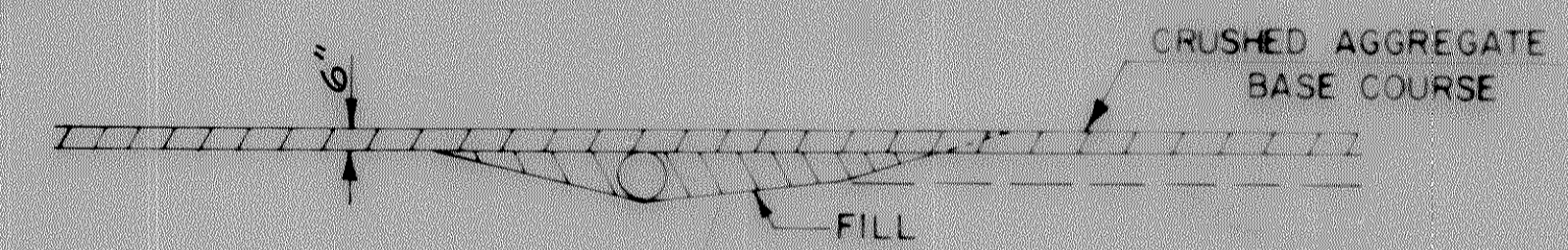
CURVE DATA IS BASED ON THE ARC DEFINITION.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

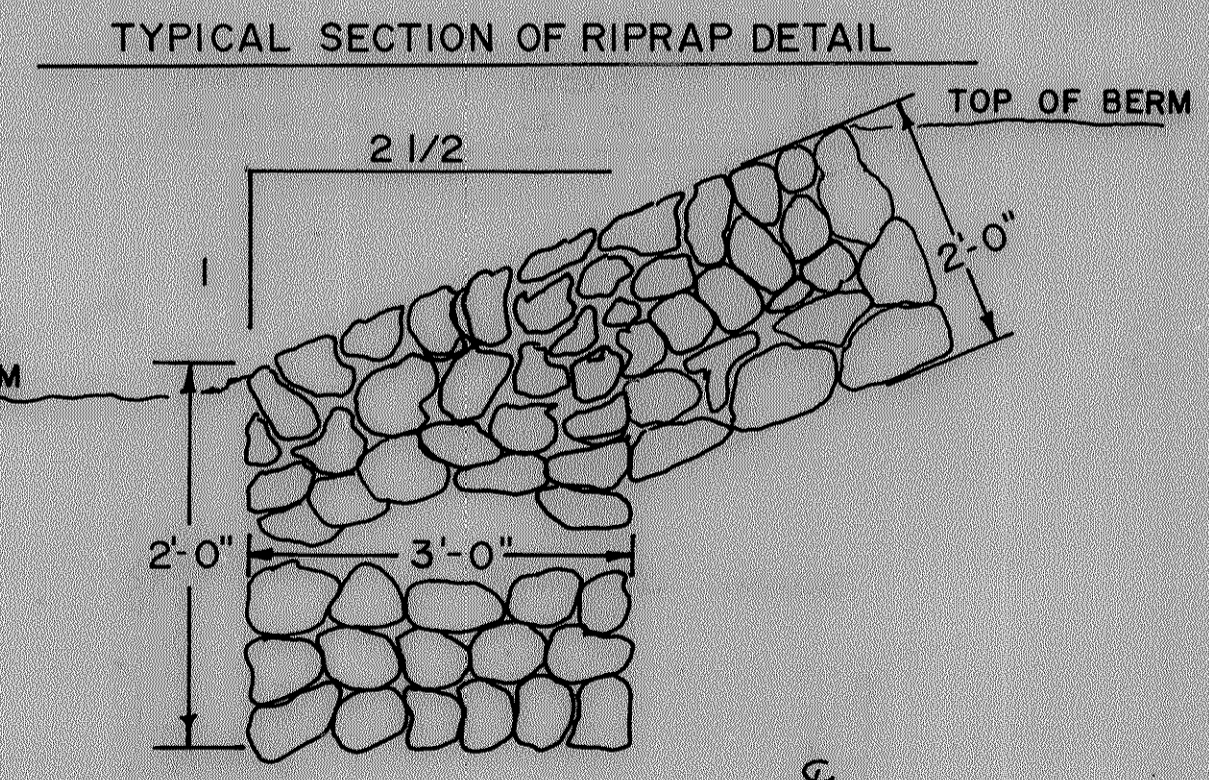
SAWING EXISTING PAVEMENT ON THIS PROJECT IS CONSIDERED INCIDENTAL TO OTHER CONTRACT ITEMS.



**PRIVATE ENTRANCE DETAIL**



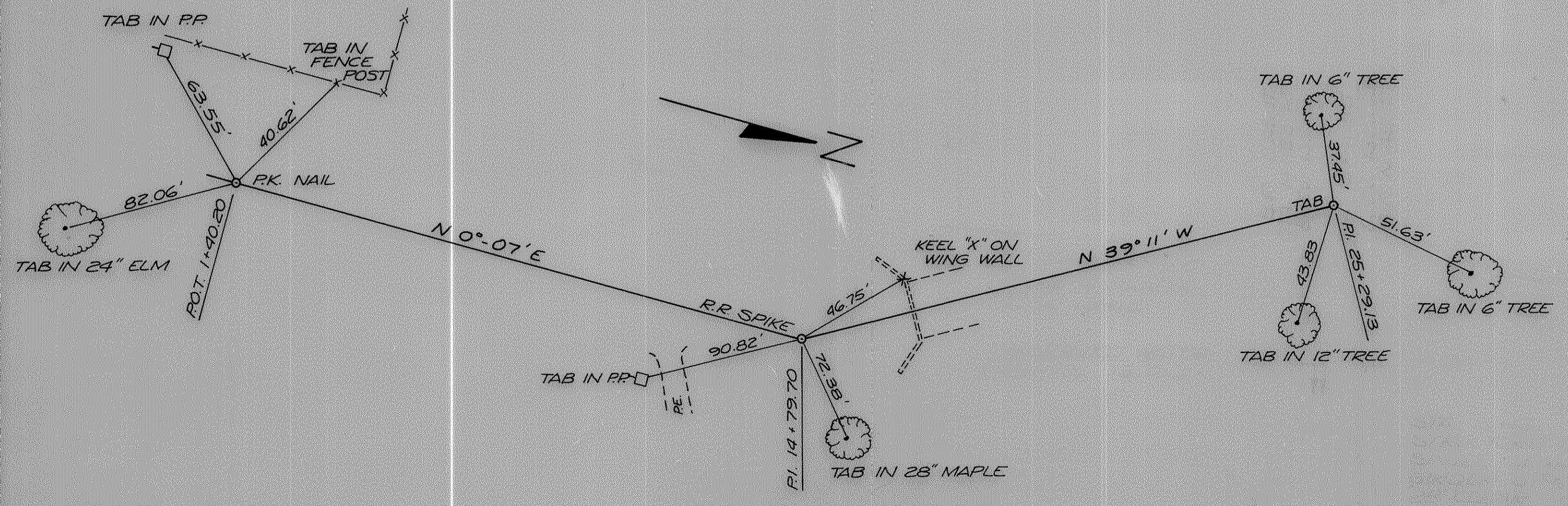
**PROFILE OF ENTRANCES**



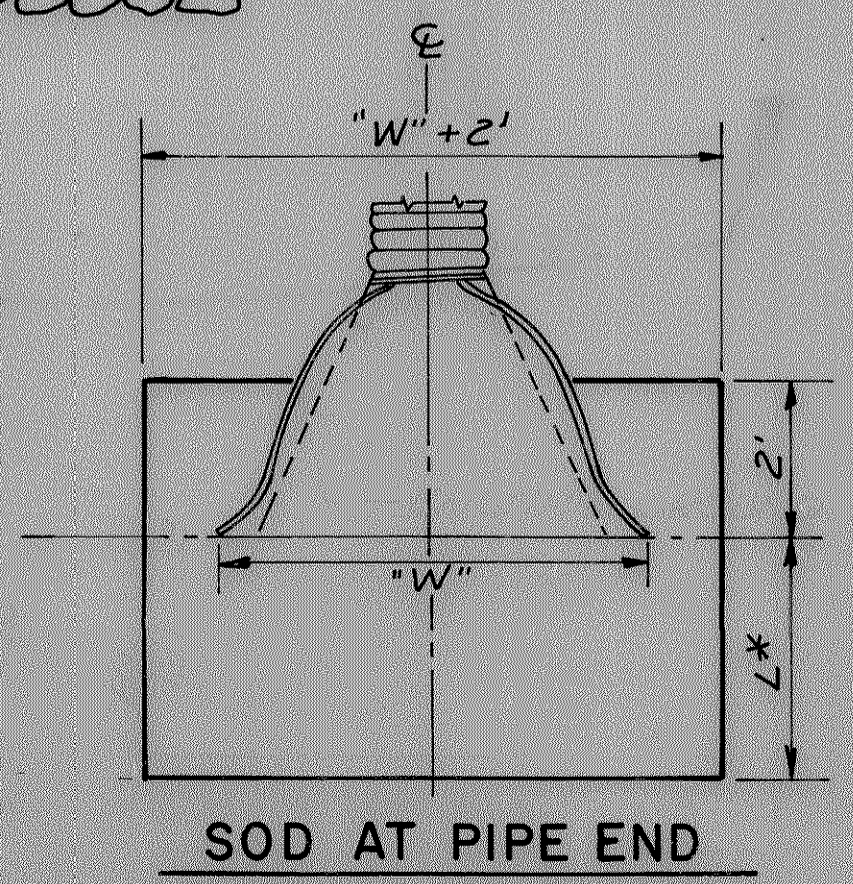
**TYPICAL SECTION OF RIPRAP DETAIL**

- STANDARD ABBREVIATIONS**
- |                       |                        |                              |
|-----------------------|------------------------|------------------------------|
| LT. = LEFT            | L.F. = LINEAR FEET     | P.I. = POINT OF INTERSECTION |
| RT. = RIGHT           | IN. = INCHES           | P.C. = POINT OF CURVE        |
| C.P. = CULVERT PIPE   | SQ. YD. = SQUARE YARDS | P.T. = POINT OF TANGENT      |
| P.P. = POWER POLE     | ELEV. = ELEVATION      | BIT. = BITUMINOUS            |
| T.P. = TELEPHONE POLE | R/W = RIGHT OF WAY     | CL = CENTERLINE              |
| Δ = DELTA ANGLE       | REQ'D = REQUIRED       | T = TRANSIT LINE             |
| D = DEGREE            | STA. = STATION         | PL. = PROPERTY LINE          |
| R = RADIUS            | V.C. = VERTICAL CURVE  |                              |
| T = TANGENT           | PE. = PRIVATE ENTRANCE |                              |
| L = LENGTH            | FE. = FIELD ENTRANCE   |                              |

- STANDARD DETAIL DRAWINGS**
- 8E8-1 TYPICAL INSTALLATIONS OF EROSION BALES
  - 8F1-9 APRON ENDWALLS FOR CULVERT PIPE AND PIPE ARCH
  - 12A3-4 NAME PLATE STRUCTURES
  - 14B2-8a & 8b CLASS "A" STEEL PLATE BEAM GUARD (TWO SHEETS)
  - 15C1-7 CONSTRUCTION BARRICADES AND STANDARD SIGNS



**ALIGNMENT DIAGRAM & CONSTRUCTION TIES**



**SOD AT PIPE END**

\* TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

**UTILITIES**

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