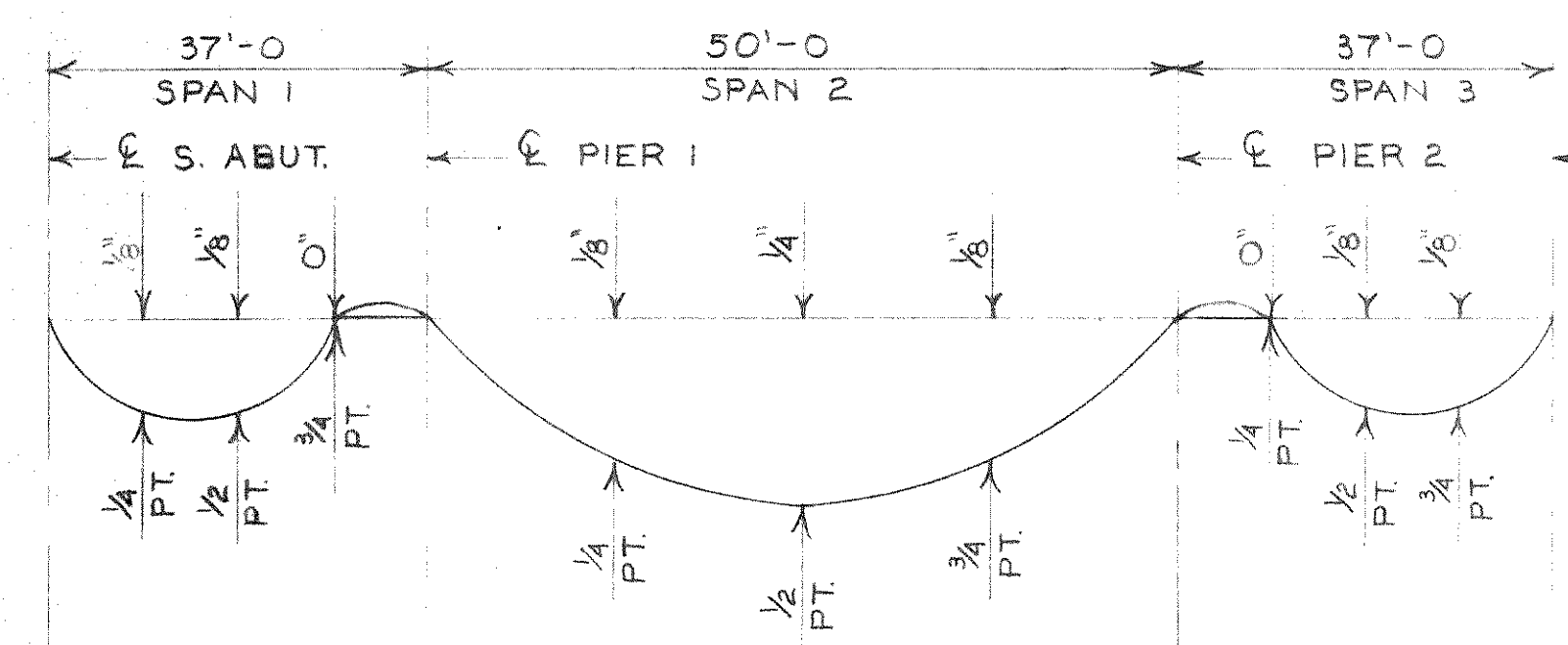


BILL OF BARS 37,500 LBS.

MARK	NO. REQ'D.	LENGTH	BENT	LOCATION
S 501	56	6'-0"	X	SLAB @ ABUTS. VERT.
S 602	34	14'-0"	X	" @ RAIL POST
S 603	68	4'-0"		" " " "
S 504	96	18'-0"	X	" " PIER HORIZ.
S 905	96	31'-9"		" - BTM. SPAN 1 & 3 LONGIT.
S 906	48	38'-0"		" " " " 2 "
S 507	83	28'-11"		" " " " 1, 2 & 3 TRANSV.
S 808	114	40'-5"		" - TOP LONGIT.
S 409	58	13'-0"		" - " SPAN 1 & 3 "
S 410	115	28'-11"		" - TOP ALL SPANS, AT PIERS & ABUTS. TRANS.
S 411	31	3'-9"	X	" @ ABUTS. VERT.
S 412	2	12'-6"		" @ S. ABUT. HORIZ.
S 413	2	17'-6"		" @ N. " " "

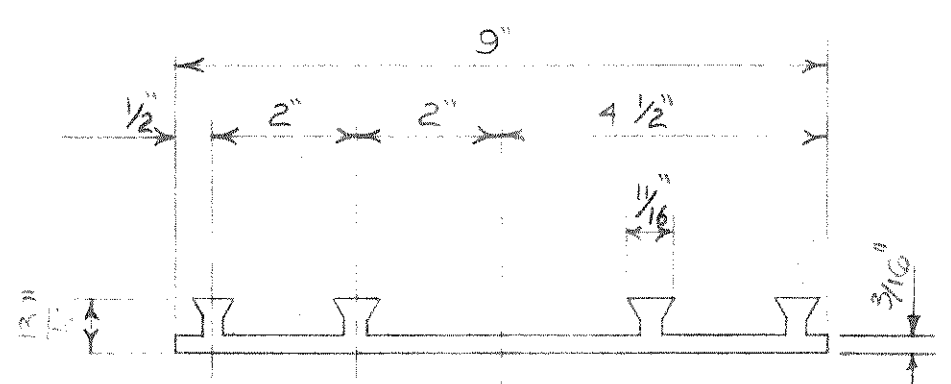
CROSS SECTION THRU ROADWAY (SECTION TAKEN NORMAL TO C OF RDWY.)



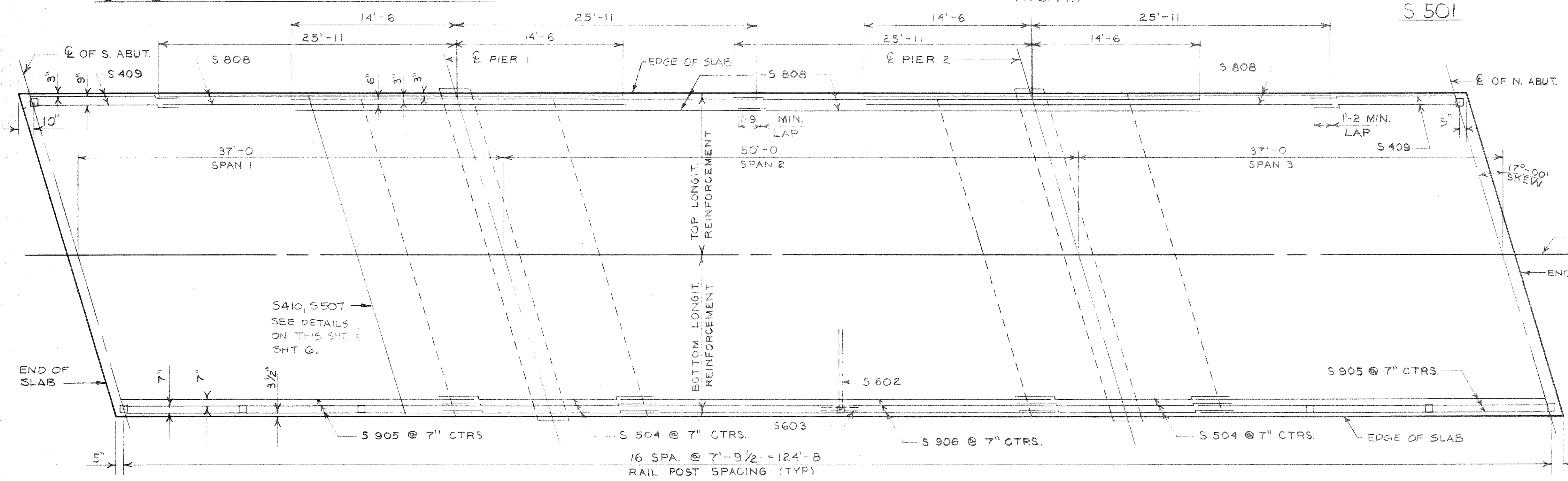
PROVIDE CAMBER OF 3/8" AT THE 1/2 PT. OF SPANS 1 & 3 AND 3/4" AT THE 1/2 PT. OF SPAN 2 TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE PLASTIC FLOW. THIS DOES NOT INCLUDE AN ALLOWANCE FOR FORM SETTLEMENT. THE PROFILE LINE OF THE C OF ROADWAY IS TO BE USED AS THE REFERENCE.

DEAD LOAD DEFLECTION DIAGRAM

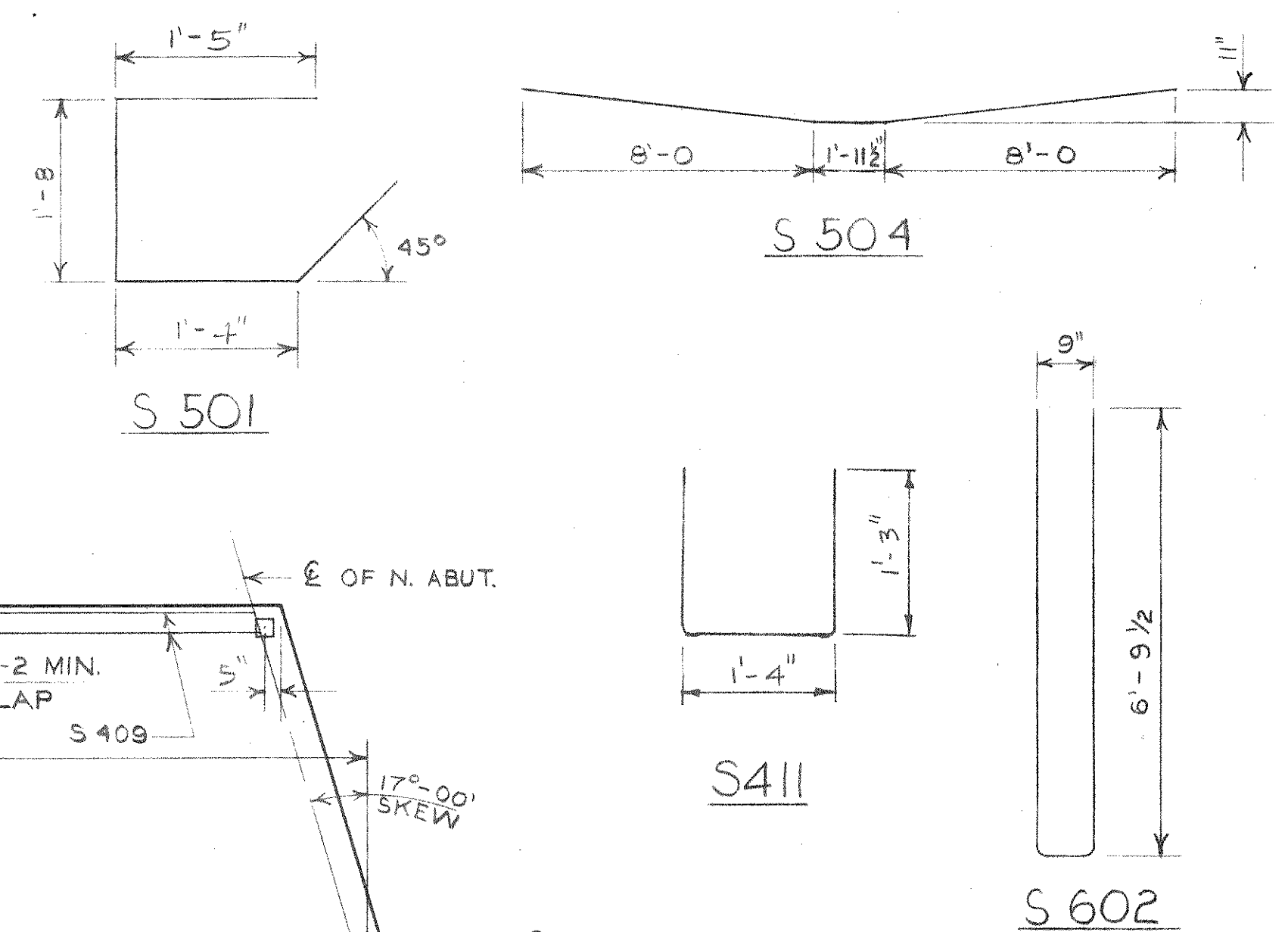
PROTECTION ANGLE DETAIL (PAID FOR AS STRUCTURAL CARBON STEEL)



POLYVINYL CHLORIDE WATERSTOP (P.C.W.)



PLAN



No.	Date	Revision	By
MEAD AND HUNT, INC. CONSULTING ENGINEERS MADISON, WISCONSIN			
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-35-77			
Const. Spec.	1975	Drawn By D.A.B.	Plans Checked R.I.S.
SUPERSTRUCTURE			SHEET 5 of 7
X 59598			